

**GENERAL NOTES**

1. AN ENCROACHMENT PERMIT SHALL BE OBTAINED FROM THE DEPARTMENT OF DEVELOPMENT SERVICES PRIOR TO THE START OF ANY CONSTRUCTION WITHIN THE LIMITS OF THE PUBLIC RIGHT-OF-WAY INCLUDING BUT NOT LIMITED TO UTILITY CONNECTIONS OR GRADING.
2. ACCEPTANCE OF THESE IMPROVEMENTS AS SHOWN DOES NOT CONSTITUTE APPROVAL OF ANY CONSTRUCTION OUTSIDE THE PROJECT BOUNDARY.
3. ALL UNDERGROUND UTILITIES WITHIN THE STREET RIGHT-OF-WAY SHALL BE CONSTRUCTED, TESTED AND CONNECTED PRIOR TO CONSTRUCTION OF BERMS, CURBS, CROSS-GUTTERS, SIDEWALKS, RETAINING WALLS OR FINAL PAVING.
4. THE EXISTENCE AND LOCATION OF EXISTING UNDERGROUND FACILITIES SHOWN ON THESE PLANS WERE OBTAINED BY A SEARCH OF AVAILABLE RECORD DATA AND FIELD SURVEYS. TO THE BEST OF OUR KNOWLEDGE, EXISTING FACILITIES ARE SHOWN ON THESE PLANS. THE CONTRACTOR IS REQUIRED TO TAKE PRECAUTIONARY MEASURES TO PROTECT ANY EXISTING FACILITIES SHOWN HEREON AND TO MAKE A REASONABLE AND DILIGENT SEARCH TO DETERMINE ANY OTHER WHICH ARE NOT OF RECORD OR NOT SHOWN ON THESE PLANS AND TO PROTECT THE SAME. ANY FACILITIES SO LOCATED SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER AND INCLUDED ON THE AS-BUILT DRAWINGS FOR THIS PROJECT.
5. LOCATION AND ELEVATION OF IMPROVEMENTS TO BE MET BY WORK TO BE DONE SHALL BE CONFIRMED BY FIELD MEASUREMENTS PRIOR TO CONSTRUCTION OF NEW WORK. CONTRACTOR SHALL BE RESPONSIBLE FOR MAKING EXPLORATORY EXCAVATIONS AND LOCATING EXISTING UNDERGROUND FACILITIES SUFFICIENTLY AHEAD OF CONSTRUCTION TO ALLOW FOR REVISIONS TO PLANS IF REVISIONS ARE NECESSARY BECAUSE OF CHANGES IN LINE OR GRADE DUE TO THE ACTUAL LOCATION OF THE EXISTING FACILITIES.
6. THE CONTRACTOR SHALL NOTIFY SDG&E PRIOR TO STARTING WORK NEAR SDG&E FACILITIES AND SHALL COORDINATE THEIR WORK WITH COMPANY REPRESENTATIVES.  
NOTICE: FOR LOCATION OF ELECTRICAL CABLES AND GAS PIPING AND APPURTENANCES CONTACT THE SAN DIEGO GAS AND ELECTRIC COMPANY. TELEPHONE: DIAL 811
7. THE CONTRACTOR SHALL NOTIFY THE AT&T TELEPHONE COMPANY PRIOR TO STARTING WORK NEAR AT&T FACILITIES AND SHALL COORDINATE THEIR WORK WITH COMPANY REPRESENTATIVES.  
NOTICE: FOR LOCATION OF CABLES AND APPURTENANCES CONTACT AT&T. TELEPHONE: DIAL 811
8. THE CONTRACTOR SHALL NOTIFY PADRE DAM MUNICIPAL WATER DISTRICT 48 HRS. PRIOR TO STARTING WORK NEAR PADRE DAM MUNICIPAL WATER DISTRICT FACILITIES AND SHALL COORDINATE THEIR WORK WITH DISTRICT REPRESENTATIVES.  
NOTICE: FOR LOCATION OF WATER AND SEWER FACILITIES AND APPURTENANCES CONTACT PADRE DAM MUNICIPAL WATER DISTRICT. TELEPHONE: DIAL 811.
9. THE CONTRACTOR SHALL NOTIFY COX CABLE TV PRIOR TO STARTING WORK NEAR COX CABLE FACILITIES AND SHALL COORDINATE THEIR WORK WITH COMPANY REPRESENTATIVES.  
NOTICE: FOR LOCATION OF CABLES AND APPURTENANCES CONTACT COX CABLE. TELEPHONE: DIAL 811
10. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT THE UTILITY AGENCIES, ADVISE THEM OF THE PROPOSED IMPROVEMENTS, AND COORDINATE RELOCATIONS, IF NEEDED.
11. POWER SOURCES AND RUNS SERVING STREET LIGHTS SHALL BE SHOWN ON THE "AS-BUILT" IMPROVEMENT DRAWINGS. ALL SOURCES SHALL BE LOCATED WITHIN THE DEDICATED RIGHT-OF-WAY, OR WITHIN EASEMENTS DEDICATED TO THE CITY OF SANTEE.
12. CITY OF SANTEE - INSPECTION REQUIREMENTS:
  - A. CONTRACTOR SHALL INFORM THE CITY PROJECT ENGINEER 48 HRS. IN ADVANCE OF COMMENCING WORK. PHONE (619) 258-4100 EXT. 168.
  - B. THE CONTRACTOR SHALL GIVE A MINIMUM OF 24 HRS. NOTICE ON REQUESTS FOR INSPECTION. PHONE (619) 258-4100 EXT. 168.
  - C. ANY WORK PERFORMED WITHOUT THE BENEFIT OF INSPECTION IS SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
13. THE CONTRACTOR SHALL RELOCATE STREET SIGNS WHEN CONDITIONS SO DICTATE. CONTRACTOR SHALL PROTECT AND ASSUME RESPONSIBILITY FOR ALL OTHER TRAFFIC CONTROL DEVICES. CONTRACTOR SHALL REPLACE TRAFFIC STRIPING, STREET MARKINGS, AND CURB MARKINGS REMOVED OR DAMAGED BY HIS WORK.
14. SIDEWALK JOINT LOCATIONS SHALL BE INSTALLED IN ACCORDANCE WITH REGIONAL STANDARD DRAWING NO. G-9. CURB OR CURB AND GUTTER JOINT LOCATIONS SHALL INCLUDE EXPANSION JOINTS AT CURB RETURNS, ADJACENT TO WALLS AND STRUCTURES AND AT 45' INTERVALS. WEAKENED PLANE JOINTS SHALL BE PLACED EVERY 15' AND GROOVED JOINTS PLACED EVERY 5'. ALL CONCRETE JOINTS SHALL CONFORM TO REGIONAL STANDARD DRAWING NO. G-10. SIDEWALKS SHALL BE REMOVED TO THE NEAREST JOINT.
15. SIDEWALKS AND BIKE PATHS ARE DESIGNED TO HAVE CLEAR UNOBSTRUCTED ACCESS. THESE FACILITIES SHALL BE KEPT CLEAR OF STREET LIGHTS, FIRE HYDRANTS, METER BOXES, TRANSFORMERS, ELECTRICAL PULL BOXES, BLOWOFFS, AIR VENTS OR OTHER SIMILAR OBSTRUCTIONS AT ALL TIMES AND SHALL MAINTAIN 4' MINIMUM CLEARANCE.
16. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING ADEQUACY OF DESIGN AND CONSTRUCTION IN ACCORDANCE WITH THE SPECIFIED GENERAL STANDARDS OF CONSTRUCTION LISTED HEREON, AND TO PROTECT THE WORK AT ALL TIMES DURING THE COURSE OF CONSTRUCTION. FAILED OR DAMAGED WORK SUCH AS CRACKED SIDEWALKS, CURB AND GUTTER, CROSS GUTTERS, DRIVEWAY APRONS AND SO FORTH, SHALL BE REPLACED TO THE NEAREST JOINT OR SCORE LINE IN EACH DIRECTION PRIOR TO PLACEMENT OF THE FINAL PAVEMENT COURSE.
17. THE CONTRACTOR SHALL ENFORCE SAFETY MEASURES AND REGULATIONS INCLUDING THE DESIGN, CONSTRUCTION AND MAINTENANCE FOR CONFORMING TO ALL LOCAL, STATE, AND FEDERAL SAFETY AND HEALTH STANDARDS, LAWS AND REGULATIONS.
18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL, DISPOSAL, AND COORDINATION OF ALL PRIVATE FACILITIES, FENCES, WALLS, DEBRIS, LANDSCAPING AND IRRIGATION WITHIN THE RIGHT OF WAY WHICH REQUIRES REMOVAL FOR CONSTRUCTION PURPOSES. EACH INDIVIDUAL OWNER EFFECTED SHALL BE NOTIFIED 5 DAYS IN ADVANCE PRIOR TO DEMOLITION OR REMOVAL OF PRIVATE PROPERTY WITHIN THE RIGHT OF WAY.
19. SOME UTILITIES MAY BE RELOCATED BY THE UTILITY COMPANIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL VERIFY THAT ALL UTILITIES ARE RELOCATED PRIOR TO START OF WORK BY CONTACTING THE CITY.
20. CONTRACTOR SHALL POSSESS A CLASS "A" LICENSE AT THE TIME OF BID SUBMISSION.
21. CONTRACTOR TO LOCATE AND ADJUST SURVEY MONUMENTS TO GRADE AND COORDINATE WITH LOCAL AGENCIES.
22. MTS GENERAL NOTES SEE SHEET 2.

**LIST OF CALTRANS STANDARD PLANS:**

RSP A20D	PAVEMENT MARKERS AND TRAFFIC LINES - TYPICAL DETAILS
A24A	PAVEMENT MARKINGS - ARROWS
A24C	PAVEMENT MARKINGS - SYMBOLS AND NUMERALS
A24D	PAVEMENT MARKINGS - WORDS
RSP A24E	PAVEMENT MARKINGS - WORDS, LIMIT AND YIELD LINES
RSP ES-1A	ELECTRICAL SYSTEMS (LEGEND)
RSP ES-1B	ELECTRICAL SYSTEMS (LEGEND)
RSP ES-1C	ELECTRICAL SYSTEMS (LEGEND)
RSP ES-8A	ELECTRICAL SYSTEMS (NON-TRAFFIC PULL BOX)
ES-11	ELECTRICAL SYSTEMS (FOUNDATION INSTALLATIONS)
ES-13A	ELECTRICAL SYSTEMS (SPlice INSULATION METHODS DETAILS)
ES-7H (2002)	SIGNAL AND LIGHTING STANDARDS CASE 5 ARM LOADING WIND VELOCITY = 129 KM/H (80MPH) ARM LENGTHS 18.2M-19.8M (60'-65')

**LIST OF SAN DIEGO REGIONAL STANDARD DRAWINGS:**

G-2	CURB AND GUTTER - COMBINED
G-7	SIDEWALK - TYPICAL SECTIONS
G-9	SIDEWALK JOINT LOCATIONS
G-10	CONCRETE JOINT DETAILS
G-14G	ASPHALT CONCRETE DRIVEWAY
G-27	CURB RAMP - TYPES A AND B (NEW CONSTRUCTION)
G-30	TRUNCATED DOMES
G-32A	GENERAL NOTES FOR CURB RAMPS
G-32B	NOTES FOR CURB RAMPS
D-2	CURB INLET - TYPE B
D-11A	INLETS AND CLEANOUTS NOTES AND DETAIL
D-11B	INLETS AND CLEANOUTS NOTES AND DETAIL
D-12	CURB INLET OPENING
D-13	WELDED STEEL GRATE FRAME
D-15	DRAINAGE STRUCTURE GRATE
D-25A	CURB OUTLET-TYPE A
D-29	CATCH BASIN-TYPE I
D-62	PIPE COLLAR
SC-01	SEWER CLEANOUT
I-15	ELECTRICAL PULLBOX FOR DIRECT BURIAL CONTROL WIRES AND SPLICING NOTES

**LIST OF CITY OF SANTEE PUBLIC WORKS STANDARDS**

PW-01	TRENCH RESURFACING
PW-02	NARROW TRENCH RESURFACING
PW-03	SLOT TRENCH RESURFACING
PW-17	METAL POLE WALKWAY LIGHTING STANDARD
PW-20	CONCRETE POLE STREET LIGHTING STANDARD
PW-21	COMMERCIAL DRIVEWAY/ALLEY APRON

**LIST OF WATER AGENCY STANDARDS**

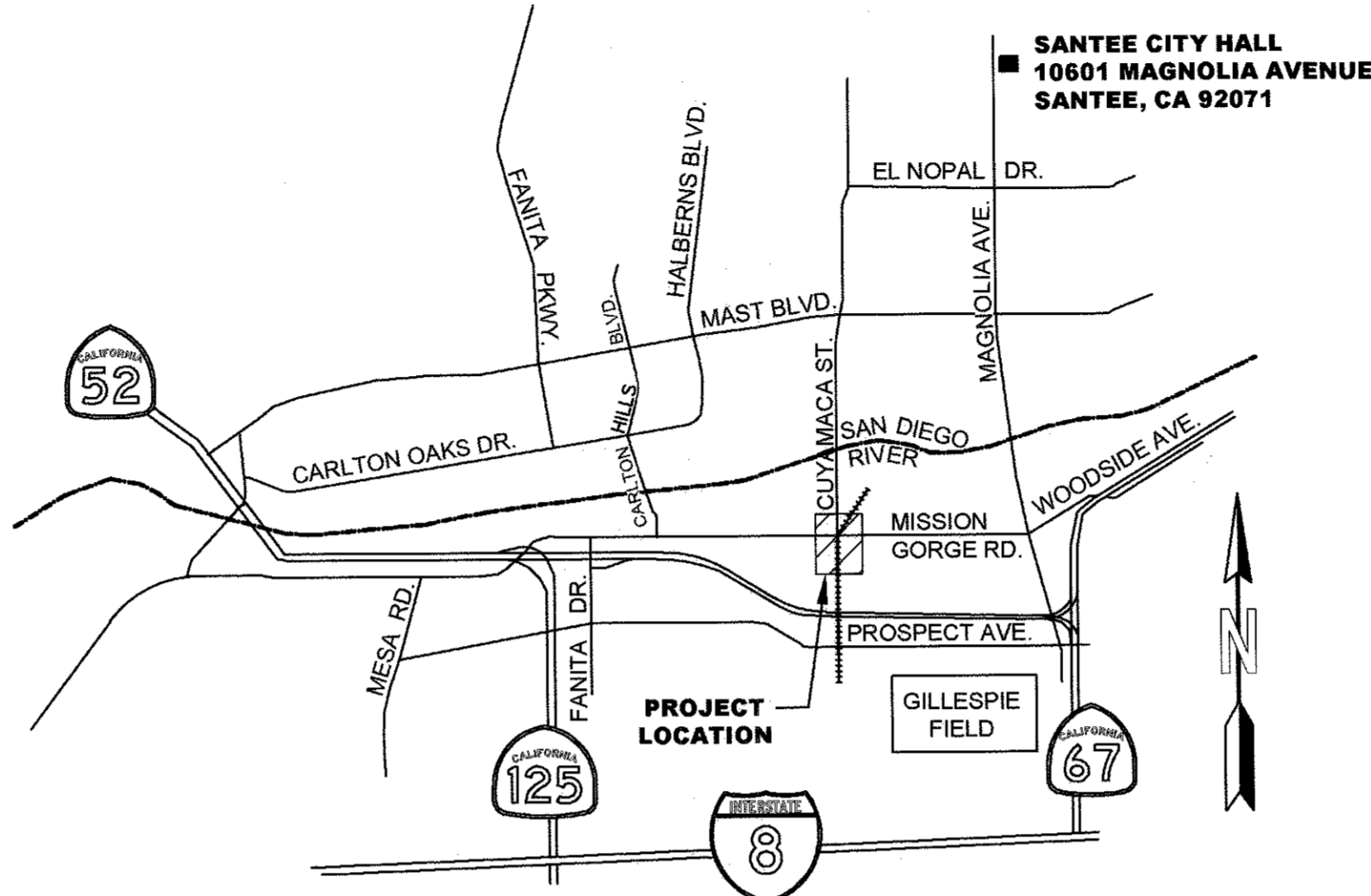
WB-01	50MM (2") BLOWOFF INSTALLATION
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CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCHMARK
CONTRACTOR						STATION 2075 PER ROS 11252 - LEAD PLUG & BRASS TAG STAMPED "RCE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST RETURN AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.
INSPECTOR						ELEVATION: 337.97, M.S.L. DATUM
DATE COMPLETED						

# CUYAMACA STREET RIGHT TURN POCKET CIP 2014-02

INTERSECTION OF CUYAMACA STREET  
AND MISSION GORGE ROAD



**VICINITY MAP**  
NO SCALE

**PROJECT LOCATION:** MISSION GORGE ROAD AND CUYAMACA STREET INTERSECTION, SANTEE, CA 92071

**LEGEND:**

\* ALL DIMENSIONS IN FEET UNLESS OTHERWISE SHOWN

ITEM (SDRSD)	SYMBOL
CENTERLINE	10+00
PROPOSED RIGHT OF WAY	---
EXIST RIGHT OF WAY	---
EXIST EASEMENT	---
EXIST TELEPHONE	---
EXIST CABLE TV	---
EXIST ELECTRIC	---
EXIST FIBER OPTIC	---
EXIST SANITARY SEWER (SIZE AND TYPE AS NOTED ON PLANS)	---
EXIST WATER	---
EXIST GAS	---
EXIST RECYCLED WATER	---
EXIST CURB INLET	---
EXIST CURB AND GUTTER	---
EXIST STORM DRAIN	---
EXIST MTS CATENARY POLE	---

EXIST MTS CATENARY WIRE	---
EXIST GUY WIRE	---
EXIST CATENARY POLES	---
EXIST STREET LIGHT	---
CONSTRUCTION NOTE	#
EXIST SIGN	---
COLD MILLING (2") AND 2" AC PAVEMENT	---
6" AC ON 12" CAB	---
4" PCC (UNLESS OTHERWISE NOTED)	---
4" AC PAVEMENT OVER NATIVE SOIL	---
SAWCUT LINE	---
DAYLIGHT/SLOPE LINE	---
TCE	---
PROPERTY BOUNDARY	---
BMP	---
ROCK COBBLE AND LANDSCAPE BOULDERS	---

**ENGINEER OF WORK'S DESIGN CERTIFICATION**

I, MARK TARRALL, HEREBY DECLARE THAT I AM THE ENGINEER OF WORK FOR THIS PROJECT, THAT I HAVE EXERCISED RESPONSIBLE CHARGE, AS DEFINED IN SECTION 6703 OF THE BUSINESS AND PROFESSIONS CODE, OVER THE ENGINEERING DESIGN OF THIS PROJECT AND THAT THE DESIGN IS CONSISTENT WITH CURRENT STANDARDS.

SIGNED: Mark Tarrall DATE: MARCH 4, 2024  
REGISTRATION NO. C71953, MY REGISTRATION EXPIRES ON 12/31/2025

**SHEET INDEX**

NO.	DESCRIPTION
1	TITLE SHEET
2	NOTES, LEGEND, AND KEY MAP
3	TYPICAL CROSS-SECTION
4	PLAN AND PROFILE
5-6	CONSTRUCTION DETAILS
7	JACK AND BORING DETAIL
8-9	DRAINAGE PLAN, PROFILE, AND DETAIL
10	UTILITY PLAN
11-12	TRAFFIC HANDLING PLAN
13	PAVEMENT DELINEATION AND SIGN PLANS
14-15	LIGHTING AND TRAFFIC SIGNAL PLANS
16-21	STRUCTURE PLANS

**BENCHMARK:**

STATION 2075 PER ROS 11252 - LEAD PLUG & BRASS TAG STAMPED "RCE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST RETURN AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE

ELEVATION: 337.97, M.S.L. DATUM

**BASIS OF BEARINGS:**

THE BASIS OF BEARINGS FOR THIS SURVEY IS THE CALIFORNIA COORDINATE SYSTEM OF 1983 (CCS83) ZONE 6, NAD83 BASED UPON THE GRID BEARING BETWEEN GPS STATIONS 2075 AND 2082 PER RECORD OF SURVEY 11252 AND IS DETERMINED BY GPS MEASUREMENTS TAKEN ON OCTOBER 2, 2019.

GRID BEARING 2075 TO 2082: N 89°35'31" E 2016.98'

STATION 2075: LEAD PLUG & BRASS TAG STAMPED "RCE 16930"

N: 1885688.621  
E: 6334933.142  
ELEV: 337.97 MSL  
CSF: 0.9999864  
CONVERGENCE: -0°24'20.29"

STATION 2082: STANDARD STREET SURVEY MONUMENT STAMPED "RCE 9416"  
N: 1885702.986  
E: 6336950.070

**STANDARD SPECIFICATIONS:**

1. STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION ("GREENBOOK") 2015 EDITION.
2. STANDARD SPECIAL PROVISIONS.
3. CITY OF SANTEE DEVELOPMENT SERVICES STANDARDS, LATEST EDITION.
4. CALIFORNIA DEPARTMENT OF TRANSPORTATION, "MANUAL OF TRAFFIC CONTROLS FOR CONSTRUCTION AND MAINTENANCE WORK ZONES," LATEST EDITION.
5. STANDARD SPECIFICATIONS OF THE PADRE DAM MUNICIPAL WATER DISTRICT, WATER AGENCIES' STANDARDS, W.A.S., LATEST EDITION
6. CALTRANS STANDARD PLANS 2002 AND 2018.

**STANDARD DRAWINGS:**

1. CITY OF SANTEE STANDARD DRAWINGS.
2. SAN DIEGO REGIONAL STANDARD DRAWINGS (SDRSD) AS RECOMMENDED BY THE REGIONAL STANDARDS COMMITTEE, MAINTAINED AND PUBLISHED BY THE SAN DIEGO COUNTY DEPARTMENT OF PUBLIC WORKS, 2018 EDITION.
3. STANDARD DRAWINGS OF THE PADRE DAM MUNICIPAL WATER DISTRICT, WATER AGENCY STANDARDS, W.A.S. (LATEST EDITION)
4. CALTRANS STANDARD SPECIFICATIONS 2002 AND 2018



**UTILITY ALERT NOTE:**

ATTENTION IS DIRECTED TO THE POSSIBLE EXISTENCE OF UNDERGROUND UTILITY FACILITIES NOT KNOWN OR IN A LOCATION DIFFERENT FROM THAT WHICH IS SHOWN ON THE PLANS OR IN THE SPECIAL PROVISIONS. THE CONTRACTOR SHALL TAKE STEPS TO ASCERTAIN THE EXACT LOCATION OF ALL UNDERGROUND FACILITIES PRIOR TO PERFORMING WORK THAT MAY DAMAGE SUCH FACILITIES OR INTERFERE WITH THEIR SERVICE. FORTY-EIGHT HOURS BEFORE EXCAVATING, THE CONTRACTOR SHALL VERIFY THE LOCATION OF UNDERGROUND FACILITIES BY CONTACTING UNDERGROUND SERVICE ALERT AT TELEPHONE 8-1-1.

**AS-BUILT**

SIGNATURE	RCE NUMBER	DATE
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**SEWER & WATER AGENCY**

**PADRE DAM MUNICIPAL WATER DISTRICT**

APPROVED BY: [Signature] Director of Engineering and Planning Date: 7/10/24  
R.C.E. No. 76593 Date: 12/31/24  
Project / File Name: \_\_\_\_\_ Job No. SA  
VALID FOR ONE YEAR FROM DATE OF SIGNATURE. SIGNATURE OF THESE PLANS IS NOT A COMMITMENT TO SERVE.

<b>CITY OF SANTEE</b>	
FIRE DEPARTMENT <u>[Signature]</u> FIRE MARSHAL	<u>7/11/24</u> DATE
COMMUNITY SERVICES DEPARTMENT <u>[Signature]</u> PUBLIC SERVICES MANAGER	<u>7-10-2024</u> DATE
ENGINEERING DIVISION <u>[Signature]</u> FOR CITY ENGINEER RCE NO. <u>60118</u> EXPIRES: 06/30/26	<u>7/10/24</u> DATE
PLANNING DIVISION <u>[Signature]</u> PROJECT PLANNER	<u>7/10/24</u> DATE
TRAFFIC ENGINEERING DIVISION <u>[Signature]</u> CITY ENGINEER	<u>7-16-24</u> DATE

<b>CITY OF SANTEE</b>		DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO.	DRAWING NO.
<b>TITLE SHEET</b>			CIP 2014-02	2021-100
<b>CUYAMACA STREET RIGHT TURN POCKET</b>				SHEET 1 OF 21

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024



**MTS GENERAL NOTES:**

- A RIGHT OF ENTRY (ROE) PERMIT IS NECESSARY WHEN ENTERING MTS / SAN DIEGO AND ARIZONA (SD&AE) RIGHT-OF-WAY (MTS R/W), INCLUDING AIRSPACE, FOR ANY PURPOSE. A ROE PERMIT IS ALSO REQUIRED WHEN WORKING IN PUBLIC RIGHT-OF-WAY OCCUPIED BY MTS / SD&AE FACILITIES. INFORMATION TO APPLY FOR ROE CAN BE OBTAINED FROM MTS'S WEBSITE AT: [HTTP://WWW.SDMTS.COM/BUSINESS/PERMITS.ASP](http://www.sdmts.com/business/permits.asp) OR CONTACT MTS PERMITS AT [MTSROW@SDMTS.COM](mailto:MTSROW@SDMTS.COM). APPLICANT/CONTRACTOR IS REFERRED TO AS THE "PERMITTEE".
- CERTIFICATION OF INSURANCE FROM YOUR INSURANCE COMPANY FOR GENERAL LIABILITY, AUTOMOBILE LIABILITY, POLLUTION LIABILITY (IF APPLICABLE), PROFESSIONAL LIABILITY (IF APPLICABLE), AND WORKMAN'S COMPENSATION MUST BE SUBMITTED AND APPROVED BY MTS BEFORE THE PERMIT WILL BE PROCESSED. FULL INSURANCE REQUIREMENT GUIDELINES CAN BE ACCESSED FROM THE MTS WEBSITE NOTED ABOVE.
- MOST GENERAL LIABILITY INSURANCE POLICIES DO NOT COVER RAILROADS. ANY EXCLUSIONS RELATING TO PERFORMANCE OF OPERATIONS WITHIN THE VICINITY OF ANY RAILROAD, BRIDGE, TRESTLE, TRACK, ROADBED, TUNNEL, UNDERPASS, OR CROSSING MUST BE DELETED FROM ALL POLICIES BY ENDORSEMENT. ADDITIONALLY, A SEPARATE RAILROAD PROTECTIVE LIABILITY POLICY WILL MOST LIKELY BE REQUIRED AFTER PROJECT REVIEW AS DEEMED NECESSARY BY MTS.
- ALL PERSONNEL PERFORMING WORK ON MTS R/W, OR ENTERING MTS R/W SHALL REQUIRE PROPER MTS RAIL SAFETY TRAINING CERTIFICATION PRIOR TO ENTERING MTS R/W. ANY CONTRACTORS OR SUBCONTRACTORS PERFORMING WORK ON BEHALF OF PERMITTEE, SHALL BE DEEMED AS AGENTS OF PERMITTEE AND SHALL REQUIRE SAID TRAINING AS WELL. FOR TRAINING INFORMATION, EMAIL [MTSROW@SDMTS.COM](mailto:MTSROW@SDMTS.COM). TRAINING CERTIFICATION IS VALID FOR ONE YEAR AND IS CONFINED TO MTS/SD&AE FACILITIES ONLY.
- PERMITTEE SHALL PROVIDE MTS WITH AN APPROVED SET OF TRAFFIC CONTROL PLANS THAT CONFORM TO THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND COMPLY IN PARTICULAR WITH PART 8 "TRAFFIC CONTROL FOR RAILROAD AND LIGHT RAIL TRANSIT GRADE CROSSINGS".
- IF PROJECT INVOLVES CASING SLEEVES UNDER RAILROAD TRACKS AND ACROSS MTS R/W, PERMITTEE SHALL ADHERE TO MTS JACK AND BORE DESIGN CRITERIA AND CONSTRUCTION NOTES.
- A PRE-CONSTRUCTION MEETING WILL BE REQUIRED WITH MTS/SDTI PRIOR TO WORK COMMENCING WITHIN MTS R/W. A WRITTEN NOTICE OF PLANNED START OF WORK MUST BE SUBMITTED TO MTS A MINIMUM OF FIVE (5) BUSINESS DAYS PRIOR TO WORK STARTING IN MTS R/W. ALL WORK WILL BE STOPPED AND PERMITTEE WILL NOT BE ALLOWED IN MTS R/W WITHOUT PROPER NOTIFICATION.
- PERMITTEE'S ON-SITE SUPERVISION SHALL RETAIN/MAINTAIN A FULLY EXECUTED COPY OF THE RIGHT OF ENTRY PERMIT AT ALL TIMES WHILE ON MTS R/W.
- SDTI RAIL FLAGGING WILL BE REQUIRED ANYTIME WORK IS WITHIN FIFTEEN (15) FEET OF ANY OPERABLE TRACK INCLUDING AIRSPACE OR AS DEEMED NECESSARY BY MTS. A SDTI FLAGPERSON / RIGHT-OF-WAY WORK REQUEST FORM MUST BE SUBMITTED TO SDTI A MINIMUM OF THREE (3) BUSINESS DAYS PRIOR TO ANTICIPATED WORK. FORMS ARE ATTACHED TO THE ROE PERMIT OR CAN BE REQUESTED THROUGH MTS RIGHT OF WAY SERVICES.
- A SDTI TRACTION POWER SHUTDOWN MAY BE NECESSARY FOR THE WORK ZONE TO PROTECT AND MAINTAIN THE REQUIRED TEN (10) FOOT CLEARANCE FROM TROLLEY OVERHEAD HIGH VOLTAGE CATENARY SYSTEM (OCS). PERMITTEE SHALL SUBMIT A SDTI RED TAG/TRACTION POWER REMOVAL REQUEST FORM TO SDTI AT LEAST THREE (3) BUSINESS DAYS PRIOR TO START OF WORK. POWER SHUTDOWNS SHALL ONLY BE ALLOWED DURING NON-OPERATING TROLLEY HOURS. FORMS ARE ATTACHED TO THE ROE PERMIT OR CAN BE REQUESTED THROUGH MTS RIGHT OF WAY SERVICES.
- PERMITTEE SHALL CONTACT AND SCHEDULE DIG-ALERT AND CABLE PIPE AND LEAK ("CPL") PRIOR TO ANY EXCAVATION IN MTS R/W. PERMITTEE SHALL NOTIFY MTS A MINIMUM OF THREE (3) BUSINESS DAYS PRIOR TO THE SCHEDULED UTILITY MARKOUT REQUEST AND SHALL SUBMIT A SDTI FLAGPERSON / RIGHT-OF-WAY WORK REQUEST FORM. SDTI PERSONNEL SHALL ACCOMPANY CPL FOR ANY MARKOUT OF TROLLEY FACILITIES.
- PERMITTEE SHALL ADHERE TO CONSTRUCTION AND SAFETY STANDARDS REQUIRED BY MTS OF THEIR CONTRACTORS WHEN WORKING WITHIN MTS R/W.
- PERMITTEE SHALL PERFORM ALL WORK IN ACCORDANCE WITH APPLICABLE CALIFORNIA PUBLIC UTILITIES COMMISSION (CPUC) AND CALIFORNIA OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (CAL-OSHA) REGULATIONS, MTS LRT DESIGN CRITERIA, AMERICAN RAILWAY ENGINEERING AND MAINTENANCE OF WAY ASSOCIATION (AREMA) STANDARD SPECIFICATIONS, MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) GUIDELINES AND MTS, SDTI AND SD&V OPERATIONS AND SAFETY POLICIES.

- PERMITTEE SHALL PERFORM ALL WORK IN ACCORDANCE WITH APPLICABLE CALIFORNIA PUBLIC UTILITIES COMMISSION (CPUC) AND CALIFORNIA OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (CAL-OSHA) REGULATIONS, MTS LRT DESIGN CRITERIA, AMERICAN RAILWAY ENGINEERING AND MAINTENANCE OF WAY ASSOCIATION (AREMA) STANDARD SPECIFICATIONS, MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) GUIDELINES AND MTS, SDTI AND SD&V OPERATIONS AND SAFETY POLICIES.
- PERMITTEE AGREES THAT NO WORK BY HIMSELF OR HIS AUTHORIZED AGENT WILL INTERFERE WITH RAILROAD/TROLLEY OPERATIONS.
- PERMITTEE AGREES TO COORDINATE ON A DAILY BASIS A REASONABLE ACCESS TO ALL MTS/SD&AE FACILITIES WITH CONTRACT OPERATORS, SDTI, AND SD&V. SDTI TROLLEY OPERATIONS ARE GENERALLY FROM THE HOURS OF 4:00 A.M. TO 2:00 A.M. THE FOLLOWING DAY. SD&V FREIGHT TRAINS NORMAL OPERATIONS ARE DURING NON-TROLLEY HOURS.
- PERMITTEE SHALL MAINTAIN SAFE PEDESTRIAN ACCESS TO ALL TROLLEY PLATFORMS AND BUS STOPS AT ALL TIMES. A MINIMUM OF FIVE (5) FOOT WIDE ACCESSIBLE PEDESTRIAN PATH THROUGH THE CONSTRUCTION SITE SHALL BE MAINTAINED AT ALL TIMES. THE CONSTRUCTION BOUNDARY SHALL CONSIST OF A TOP AND BOTTOM RAIL CONSTRUCTED OF PLASTIC PIPE, OSHA PLASTIC MESH, OR APPROVED EQUAL. YELLOW CAUTION TAPE IS NOT ACCEPTABLE.
- PERMITTEE SHALL NOT STORE EQUIPMENT, TOOLS, AND MATERIALS WITHIN FIFTEEN (15) FEET FROM TROLLEY OPERABLE TRACK AND WITHIN TWENTY-FIVE (25) FEET FROM THE FREIGHT TRACK OPERATIONS.
- PERMITTEE SHALL NOT USE OR STORE HAZARDOUS SUBSTANCES, AS DEFINED BY THE COMPREHENSIVE ENVIRONMENTAL RESPONSE, COMPENSATION, AND LIABILITY ACT, AS AMENDED ("CERCLA") OR PETROLEUM OR OIL AS DEFINED BY APPLICABLE ENVIRONMENTAL LAWS ON MTS R/W.
- NO VEHICULAR CROSSING OVER TRACKS SHALL BE INSTALLED OR USED BY PERMITTEE WITHOUT PRIOR WRITTEN PERMISSION OF RAILROAD.
- A WRITTEN NOTICE SHALL BE SUBMITTED TO MTS ONE (1) BUSINESS DAY AFTER WORK IS COMPLETED WITHIN MTS R/W. ANY ADDITIONAL WORK REQUIRED TO REPLACE OR REPAIR THE RAILROAD FACILITIES IN GOOD WORKING ORDER WILL BE THE PERMITTEE'S RESPONSIBILITY PRIOR TO RELIEF FROM MAINTENANCE WITHIN THE PERMIT AREA.
- PERMITTEE SHALL REMOVE ALL OF PERMITTEE'S TOOLS, EQUIPMENT, AND MATERIALS FROM RAILROAD PREMISES PROMPTLY UPON COMPLETION OF WORK AND SHALL RESTORE ALL FACILITIES, IMPROVEMENTS, LANDSCAPING, ETC., TO THEIR ORIGINAL CONDITION OR AS SHOWN ON PROJECT WORK SITE PLANS.

**MTS JACK AND BORE DESIGN CRITERIA**

NOTE: FOR THE PURPOSES OF THIS DESIGN CRITERIA AND SUBSEQUENT CONSTRUCTION NOTES, THE TERM "JACK AND BORE" IS USED GENERALLY TO REFER TO SEVERAL TRENCHLESS CONSTRUCTION METHODS - A FEW OF WHICH ARE DESCRIBED IN THESE NOTES. THE LIMITATIONS OF THESE NOTES DO NOT PRECLUDE THE USE OF OTHER TRENCHLESS CONSTRUCTION METHODS, PROVIDED THEY ARE APPROVED BY MTS.

THE BASIC GUIDELINES TO BE FOLLOWED FOR JACK AND BORE DESIGN ARE REFERENCED IN THE AREMA (AMERICAN RAILWAY ENGINEERING AND MAINTENANCE OFWAY ASSOCIATION) MANUAL. AS AN MTS SUPPLEMENT TO THE AREMA, THE FOLLOWING STANDARDS AND DESIGN CRITERIA SHALL ALSO APPLY:

- ALL UNDERGROUND UTILITIES UNDER RAILROAD TRACKS SHALL BE ENCASED IN A LARGER PIPE OR CONDUIT CALLED THE "CASING PIPE."
- INSTALLATION OF CASING PIPES BY OPEN TRENCH IS PROHIBITED UNLESS APPROVED BY MTS.
- CASING PIPE SHALL BE INSTALLED ACROSS THE ENTIRE WIDTH OF THE RAILROAD RIGHT-OF-WAY AND SHALL EXTEND BEYOND THE RIGHT-OF-WAY A MINIMUM OF 10 FEET UNLESS OTHERWISE APPROVED BY MTS.
- THE TOP OF CASING SHALL HAVE A MINIMUM DEPTH OF 5.5 FEET BELOW THE TOP OF TIE AND A MINIMUM DEPTH 3 FEET BELOW GROUND SURFACE INCLUDING BOTTOM OF DITCHES AND OTHER LOW POINTS WITHIN THE RAILROAD RIGHT-OF-WAY.
- ALL CASING PIPES SHALL BE INSTALLED WITH A MINIMUM SLOPE OF 1% UNLESS OTHERWISE APPROVED BY MTS AND/OR OTHERWISE APPROVED BY OTHER JURISDICTIONAL DESIGN CRITERIA.
- THE ENGINEER/CONTRACTOR SHALL SUBMIT TO MTS FOR REVIEW, LOAD CALCULATIONS FOR THE PROPOSED CASING WITH APPLIED LOAD AS DEFINED BY COOPER E-80 WITH A 50% ADDED IMPACT LOAD. THE CALCULATIONS SHALL BE SIGNED AND STAMPED BY A CALIFORNIA LICENSED CIVIL OR STRUCTURAL ENGINEER.

**MTS JACK AND BORE CONSTRUCTION NOTES:**

- SEE MTS STANDARD CONSTRUCTION NOTES FOR ADDITIONAL REQUIREMENTS.
- FOR PIPELINES CARRYING FLAMMABLE OR HAZARDOUS MATERIALS, THE CONTRACTOR SHALL ADHERE TO AREMA AND REGULATORY AGENCY GUIDELINES.
- THE CONTRACTOR SHALL OBTAIN ALL PERMITS NECESSARY FOR THE PROPOSED PROJECT INCLUDING BUT NOT LIMITED TO ENCROACHMENT, SWPPP, AND ENVIRONMENTAL PERMITS, AND THIRD-PARTY UTILITY PERMITS.
- PRIOR TO ANY COMMENCEMENT OF WORK, THE CONTRACTOR SHALL SUBMIT TO MTS FOR REVIEW AND APPROVAL. AN UPDATED DESCRIPTION OF THE WORK PROCESS INCLUDING ALL PLAN AND FIELD CHANGES/MODIFICATIONS, CERTIFICATES OF COMPLIANCE, AND SCHEDULED ACTIVITIES FOR WORK AFFECTING RAILROAD RIGHT-OF-WAY.
- PRIOR TO ANY COMMENCEMENT OF WORK, THE CONTRACTOR SHALL SUBMIT TO MTS FOR REVIEW AND APPROVAL, SHORING DRAWINGS AND CALCULATIONS. ALL DRAWINGS AND CALCULATIONS SHALL BE SIGNED AND STAMPED BY A CIVIL OR STRUCTURAL ENGINEER LICENSED IN THE STATE OF CALIFORNIA.
- THE CONTRACTOR SHALL VERIFY AND MARK OUT ALL RAILROAD RIGHT OF WAY LIMITS AND PROPERTY ENTITLEMENTS (EASEMENTS, LICENSE AGREEMENTS, ETC.) WITHIN THE VICINITY OF THE WORK AND/OR RELATED TO THE WORK.
- PRIOR TO ANY COMMENCEMENT OF WORK, THE CONTRACTOR SHALL LAYOUT THE PROPOSED JACKING AND RECEIVING PITS FOR MTS INSPECTION AND ACCEPTANCE. BOTH PITS SHALL BE CONSTRUCTED OUTSIDE OF THE RAILROAD RIGHT-OF-WAY UNLESS OTHERWISE APPROVED BY MTS.
- THE CONTRACTOR SHALL MONITOR RAIL ELEVATIONS PRIOR TO, DURING, AND IMMEDIATELY AFTER THE JACK AND BORE OPERATION. BOTH RAILS OF EACH TRACK SHALL BE MONITORED AT THE POINT OF CROSSING, AND AT 10' AND 20' OUT IN EACH DIRECTION FROM THE CROSSING POINT, UNLESS OTHERWISE DIRECTED BY MTS. TOP OF RAIL ELEVATIONS SHALL BE RECORDED WITH AN ACCURACY OF ONE (1) ONE-HUNDREDTH OF A FOOT (0.01'), AND SHALL BE SUBMITTED TO MTS FOR REVIEW. SURVEYS SHALL BE CONDUCTED:
  - PRIOR TO CONSTRUCTION TO ESTABLISH A BASELINE, NO GREATER THAN 2 WEEKS BEFORE START OF CONSTRUCTION.
  - THREE TIMES A DAY FOR THE DURATION OF THE CONSTRUCTION.
  - ONCE PER DAY FOR 1 WEEK FOLLOWING 'STABLE' CONDITION WITH COMPLETION OF ALL CONSTRUCTION ACTIVITIES. IF NO MOVEMENT, NO FURTHER MONITORING IS NEEDED.
- MTS MAY REQUIRE MORE FREQUENT MONITORING AT CERTAIN LOCATIONS SUCH AS BRIDGE AND GRADE CROSSING APPROACHES, OR LOCATIONS WITH SOFT SOIL CONDITIONS.
- ALL AREAS BACKFILLED WITHIN THE RAILROAD RIGHT-OF-WAY, OR THAT ENCR OACH WITHIN A 2:1 DOWNWARD SLOPING LINE FROM THE EDGE OF RIGHT-OF-WAY, SHALL BE AT 90% RELATIVE DRY COMPACTION. BACKFILLING IN ALL OTHER AREAS SHALL BE IN ACCORDANCE WITH THE LOCAL JURISDICTION'S STANDARDS. THE CONTRACTOR SHALL SUBMIT A COMPACTION REPORT PREPARED BY A CALIFORNIA LICENSED GEOTECHNICAL ENGINEER.
- THE CONTRACTOR SHALL REMOVE ALL TEMPORARY FACILITIES CONSTRUCTED ON THE RAILROAD RIGHT-OF-WAY, IN ADDITION TO DEBRIS, TRASH, AND OTHER ITEMS NOT ORIGINALLY AT THE SITE PRIOR TO CONSTRUCTION, AND SHALL NOTIFY MTS WITHIN 24 HOURS THAT ALL CONSTRUCTION HAS BEEN COMPLETED.

**JACK AND BORE OPERATIONS**

- PRIOR TO ANY COMMENCEMENT OF WORK, THE CONTRACTOR SHALL SUBMIT TO MTS FOR REVIEW AND APPROVAL, A PLAN SHOWING THE PROPOSED METHOD OF CASING INSTALLATION, CONSTRUCTION ACCESS, STOCKPILE LOCATIONS, SWPPP CONTROL MEASURES, FENCING TYPE AND LOCATION, AND A MILESTONE SCHEDULE. THE CONTRACTOR MAY UTILIZE ANY ONE OF THE FOLLOWING METHODS OF INSTALLING CASING AND SHALL ADHERE TO ITS RESPECTIVE REQUIREMENTS:
  - THIS METHOD SHALL BE IN ACCORDANCE WITH THE AREMA (AMERICAN RAILWAY ENGINEERING AND MAINTENANCE OFWAY ASSOCIATION) MANUAL, VOLUME 1, CHAPTER 1, PART "EARTH BORING AND JACKING CULVERT PIPE THROUGH FILLS." THIS OPERATION SHALL BE CONDUCTED WITHOUT HANDMINING AHEAD OF THE PIPE AND WITHOUT THE USE OF ANY TYPE OF BORING, AUGERING, OR DRILLING EQUIPMENT.
  - BRACING AND BACKSTOPS SHALL BE DESIGNED AND JACKS WITH SUFFICIENT RATING USED SO THAT THE JACKING CAN PROGRESS WITHOUT STOPPAGE (EXCEPT FOR ADDING LENGTHS OF PIPE) UNTIL THE LEADING EDGE OF THE PIPE REACHES THE RECEIVING PIT.
  - DURING JACKING, AN EARTH PLUG 1.5 TIMES THE DIAMETER OF THE CASING SHALL BE MAINTAINED AT ALL TIMES. JACKING OPERATIONS SHALL BE CONTINUOUS ON A 24-HOUR PER DAY BASIS UNTIL THE JACKING OPERATION IS COMPLETED.

**ABBREVIATIONS:**

AB	AGGREGATE BASE	E	EAST	LL	LANE LINE	PT	EMPTY CONDUIT WITH PULL TAPE
AC	ASPHALT CONCRETE PAVEMENT	EA	EACH	LT	LEFT	PVC	POLYVINYL CHLORIDE
ACP	ASBESTOS CEMENT PIPE	ECR	END CURB RETURN	MH	MANHOLE	R	RADIUS
ADA	AMERICAN DISABILITIES ACT	EG	EXISTING GRADE	MAX	MAXIMUM	RCP	REINFORCED CONCRETE PIPE
BCR	BEGIN CURB RETURN	ELEV	ELEVATION	MIN	MINIMUM	RCPT	RECEPTACLE
BEG	BEGIN	EP	EDGE OF PAVEMENT	MISC	MISCELLANEOUS	RSP	REVISED STANDARD PLAN
BM	BENCHMARK	EXIST	EXISTING	MOD	MODIFIED	RT	RIGHT
BMP	BEST MANAGEMENT PRACTICE	FG	FINISHED GRADE	N	NORTH	RW	RECYCLED WATER
BOP	BOTTOM OF PIPE	FS	FINISHED SURFACE	NO.	NUMBER	R/W	RIGHT OF WAY
BSM	BIOFILTRATION SOIL MEDIA	FL	FLOW LINE	NTS	NOT TO SCALE	S	SEWER SERVICE -OR- SLOPE
BSW	BACK OF SIDEWALK	FSW	FRONT OF SIDEWALK	O.C.	ON CENTER	-OR-	SOUTH
C	CONDUIT	FT	FEET	OD	OUTSIDE DIAMETER	SC	SAWCUT
CC	CROWN CASTLE	G	GROUND	OG	ORIGINAL GROUND	SCH.	SCHEDULE
C&G	CURB & GUTTER	CB	GRADE BREAK	PVMT	PAVEMENT	SD	STORM DRAIN
CAB	CRUSHED AGGREGATE BASE	CL	CUTTER LIP	MIN	MINIMUM	SDG&E	SAN DIEGO GAS & ELECTRIC
CCP	CONCRETE CYLINDER PIPE	GV	GATE VALVE	MISC	MISCELLANEOUS	SDRSD	SAN DIEGO COUNTY REGIONAL STANDARD DRAWING
CL	CENTERLINE	HH	HAND HOLE	MOD	MODIFIED	SF	SQUARE FEET
CKT	CIRCUIT	HI	HIGH POINT	N	NORTH	SG	SUBGRADE
CONT	CONTINUOUS	HMA	HOT MIX ASPHALT	NO	NUMBER	SL	STREET LIGHT
CITY	CITY OF SANTEE	HP	HIGH PRESSURE	NTS	NOT TO SCALE	SM	SEWER MANHOLE
COX	COX COMMUNICATIONS	IN	INCHES	O.C.	ON CENTER	STA	STATION
CSP	CALTRANS STANDARD PLANS	IE	INVERT ELEVATION	OG	ORIGINAL GROUND	STD	STANDARD
CT	CALTRANS	IOD	IRREVOCABLE OFFER OF	OG	ORIGINAL GROUND	STL	STEEL
CY	CUBIC YARDS		DEDICATION	PVMT	PAVEMENT	SW	SIDEWALK
DIA	DIAMETER	Jt	JOINT	PB	PULL BOX	TC	TOP OF CURB
DWG	DRAWING	L	LENGTH	PCC	PORTLAND CEMENT CONCRETE	TEL	TELECOMMUNICATIONS
DWY	DRIVEWAY	LF	LINEAR FEET		-OR-	TYP	TYPICAL
				PDMWD	POINT ON COMPOUND CURVE	VAR	VARIABLE
					PADRE DAM MUNICIPAL WATER DISTRICT	VCP	VITRIFIED CLAY PIPE
				PE	POLYETHYLENE	W	WIDTH -OR- WEST
				PI	POINT OF INFLECTION	W/	WITH
				PIP	POUR IN PLACE	WAS	WATER AGENCIES' STANDARD
				P/L	PROPERTY LINE	WM	WATER METER
				PP	POWER POLE	WQ	WATER QUALITY
				PRC	POINT ON REVISE CURVE	WS	WATER SERVICE
				PROP	PROPOSED	X-ING	CROSSING



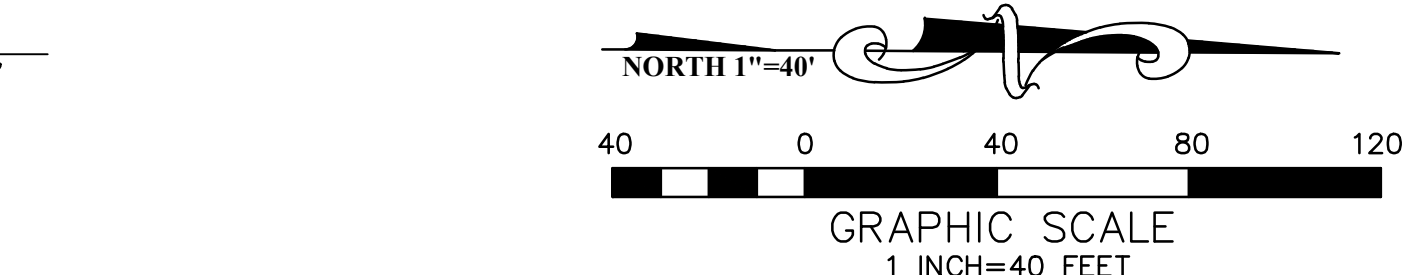
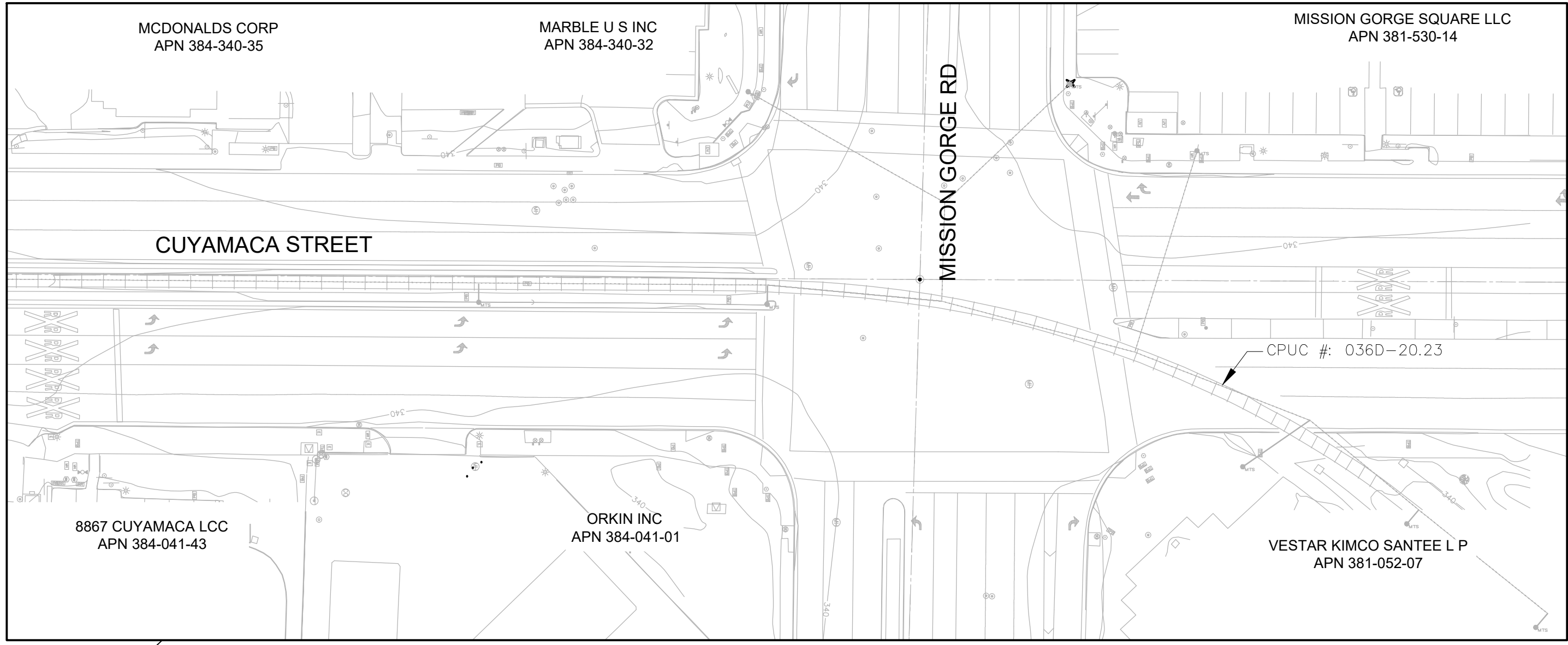
CONTRACTOR	INSPECTOR	DATE COMPLETED

CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD

STATION 2015 PER R08 1132 - LEAD PLUG & BRASS TAG STAMPED "RCE 16939" PER MAP 9372, LOCATED AT THE SOUTHEAST RETURN AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.	ELEVATION: 337.97, M.S.L. DATUM
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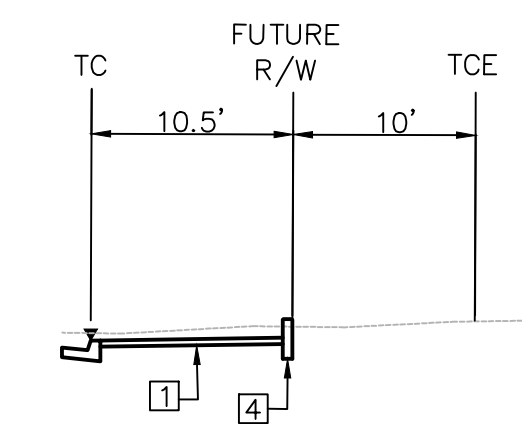
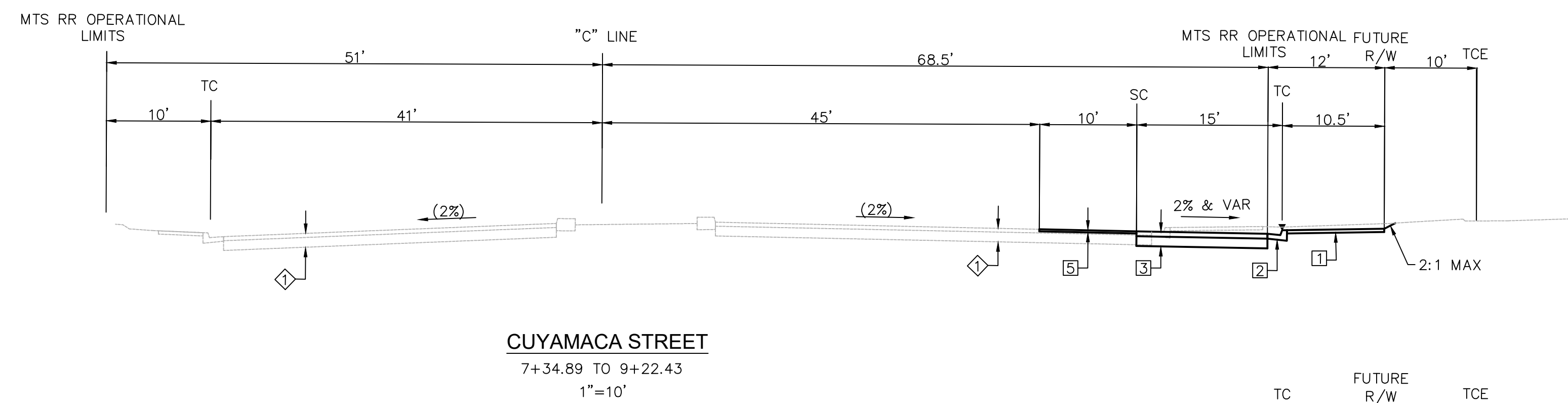
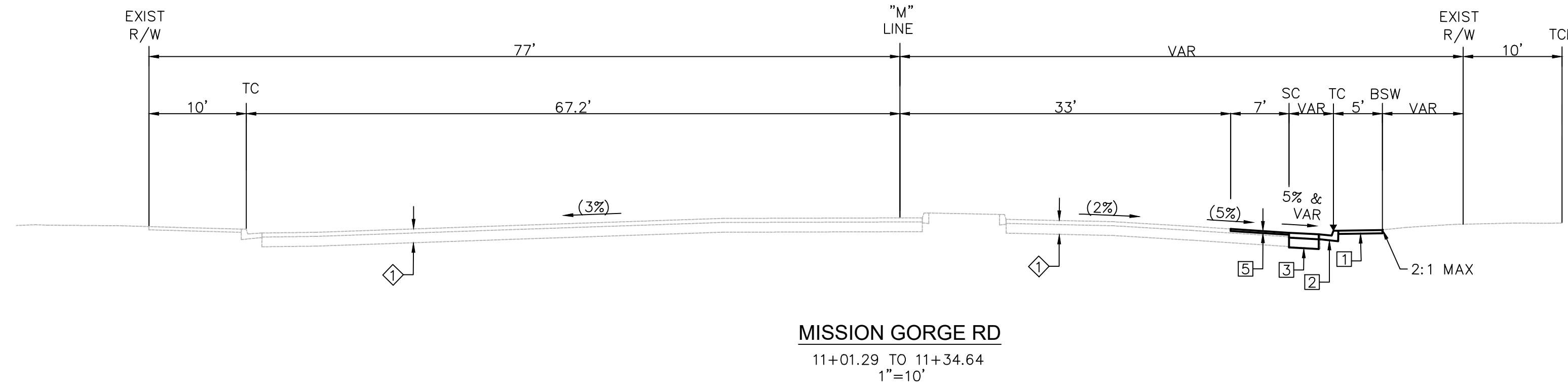
SCALE	DESIGNED BY	DRAWN BY	CHECKED BY
HORIZ: 1"=40'	VINCENT MALONG	VINCENT MALONG	MARK TARRALL
VERT: N/A	PLANS PREPARED UNDER THE SUPERVISION OF		
	MARK TARRALL DATE 01/12/2024		
	RCE NO. 71953	EXPIRES	12/31/2025

REVIEWED	BY	PROJECT ENGINEER

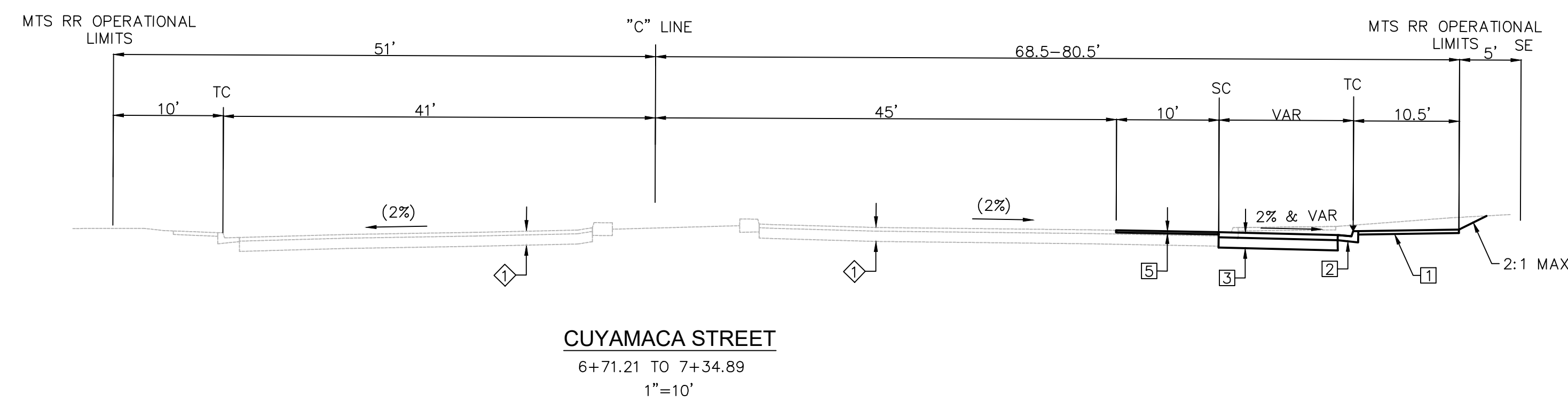


CITY OF SANTEE	DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO.	DRAWING NO.
KEY MAP AND LINE INDEX	CUYAMACA STREET RIGHT TURN POCKET	CIP 2014-02	2021-101
			SHEET 2 OF 21

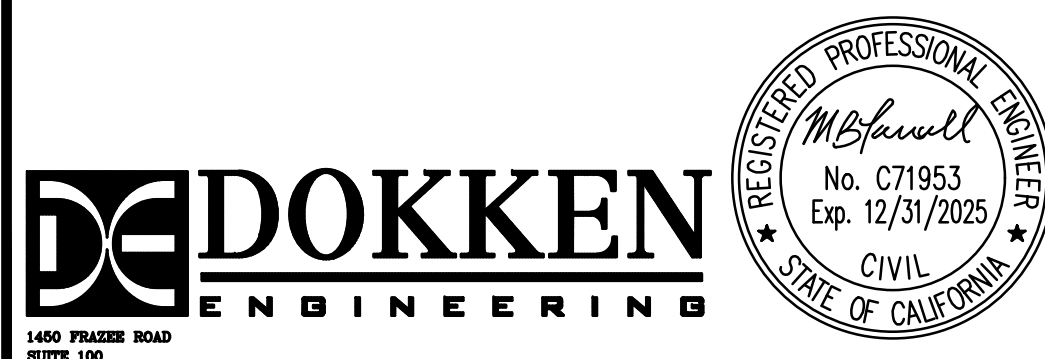
100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024



**CUYAMACA STREET**  
7+65.21 TO 7+88.40  
8+36.90 TO 8+65.20  
1"=10'



PROPOSED PAVEMENT STRUCTURAL SECTIONS		
DESCRIPTION	TYPE	
1	PCC SIDEWALK	SDRSD G-7
2	CURB AND GUTTER	SDRSD G-2 (TYPE G)
3	NEW PAVEMENT SECTION	6" AC 12" CAB
4	RETAINING CURB (MOD)	SEE SHEET 5
5	COLD MILLING (2")	2" MILL AC PVMT 2" AC PVMT
EXISTING PAVEMENT STRUCTURAL SECTIONS		
DESCRIPTION	TYPE	
EXISTING PAVEMENT	5" AC 12" CAB	



CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD

**BENCH MARK**  
STATION 2075 PER RDS 11252. LEAD PLUG & BRASS TAG STAMPED "RCE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.  
ELEVATION: 337.97, M.S.L. DATUM

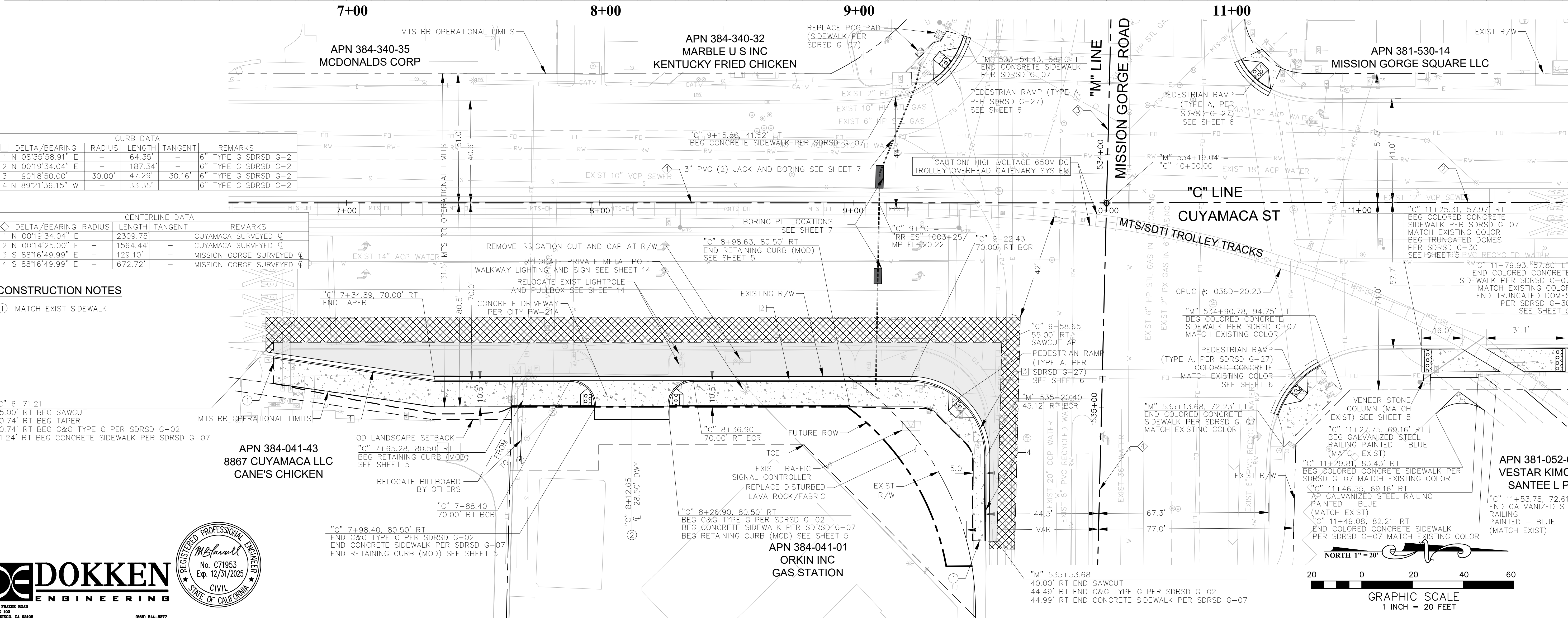
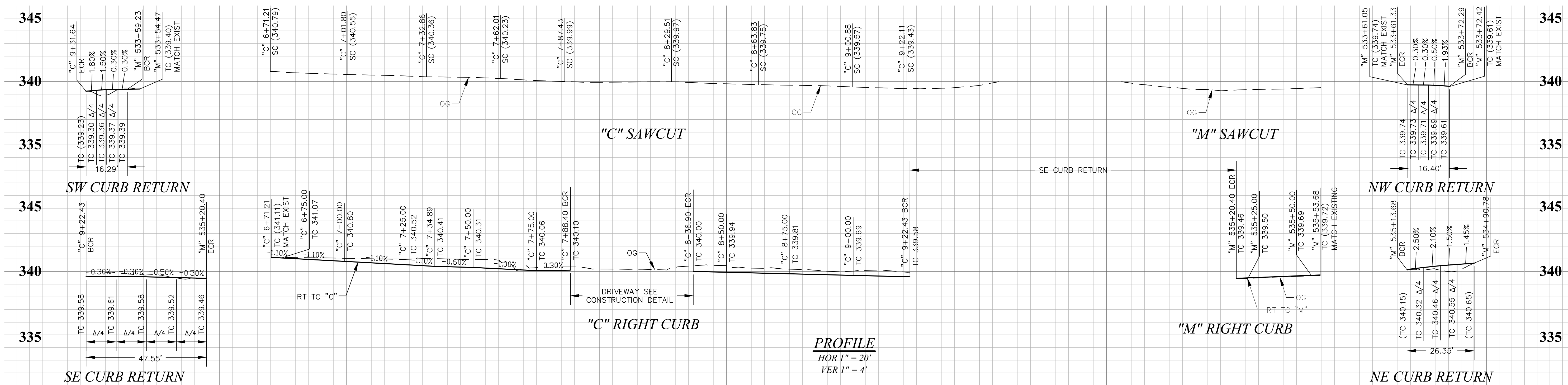
SCALE	DESIGNED BY	DRAWN BY	CHECKED BY
HORIZ: 1"=10' VERT: N/A	VINCENT MALONG	VINCENT MALONG	MARK TARRALL
	PLANS PREPARED UNDER THE SUPERVISION OF MARK TARRALL DATE 01/12/2024 RCE NO. 71953 EXPIRES 12/31/2025		

REVIEWED \_\_\_\_\_  
BY \_\_\_\_\_ PROJECT ENGINEER

CITY OF SANTEE DEPARTMENT OF DEVELOPMENT SERVICES  
TYPICAL CROSS SECTION  
CUYAMACA STREET RIGHT TURN POCKET

CITY W.O. NO. CIP 2014-02  
DRAWING NO. 2021-102  
SHEET 3 OF 21





**CURB DATA**

NO	DELTA/BEARING	RADIUS	LENGTH	TANGENT	REMARKS
1	N 08°35'58.91" E	-	64.35'	-	6" TYPE G SDRSD G-2
2	N 00°19'34.04" E	-	187.34'	-	6" TYPE G SDRSD G-2
3	S 88°16'49.99" E	30.00'	47.29'	30.16'	6" TYPE G SDRSD G-2
4	N 89°21'36.15" W	-	33.35'	-	6" TYPE G SDRSD G-2

**CENTERLINE DATA**

NO	DELTA/BEARING	RADIUS	LENGTH	TANGENT	REMARKS
1	N 00°19'34.04" E	-	2309.75'	-	CUYAMACA SURVEYED C
2	N 00°14'25.00" E	-	1564.44'	-	CUYAMACA SURVEYED C
3	S 88°16'49.99" E	-	129.10'	-	MISSION GORGE SURVEYED C
4	S 88°16'49.99" E	-	672.72'	-	MISSION GORGE SURVEYED C

**CONSTRUCTION NOTES**

- ① MATCH EXIST SIDEWALK

"C" 6+71.21  
55.00' RT BEG SAWCUT  
60.74' RT BEG TAPER  
60.74' RT BEG C&G TYPE G PER SDRSD G-02  
61.24' RT BEG CONCRETE SIDEWALK PER SDRSD G-07

APN 384-041-43  
8867 CUYAMACA LLC  
CANE'S CHICKEN

IOD LANDSCAPE SETBACK  
"C" 7+65.28, 80.50' RT  
BEG RETAINING CURB (MOD)  
SEE SHEET 5

RELOCATE BILLBOARD  
BY OTHERS  
"C" 7+88.40  
70.00' RT BCR

"C" 7+98.40, 80.50' RT  
END C&G TYPE G PER SDRSD G-02  
END CONCRETE SIDEWALK PER SDRSD G-07  
END RETAINING CURB (MOD) SEE SHEET 5

"C" 8+26.90, 80.50' RT  
BEG C&G TYPE G PER SDRSD G-02  
BEG CONCRETE SIDEWALK PER SDRSD G-07  
BEG RETAINING CURB (MOD) SEE SHEET 5

APN 384-041-01  
ORKIN INC  
GAS STATION

"C" 9+58.65  
55.00' RT  
SAWCUT AP

PEDESTRIAN RAMP  
(TYPE A, PER  
SDRSD G-27)  
SEE SHEET 6

"M" 535+20.40  
45.12' RT ECR

EXIST 20" COP WATER  
EXIST 6" PVC RECYCLED WATER

EXIST 36" WATER

"M" 535+53.68  
40.00' RT END SAWCUT  
44.49' RT END C&G TYPE G PER SDRSD G-02  
44.99' RT END CONCRETE SIDEWALK PER SDRSD G-07

"M" 534+90.78, 94.75' LT  
BEG COLORED CONCRETE  
SIDEWALK PER SDRSD G-07  
MATCH EXISTING COLOR

PEDESTRIAN RAMP  
(TYPE A, PER  
SDRSD G-27)  
SEE SHEET 6

"M" 535+13.68, 72.23' LT  
END COLORED CONCRETE  
SIDEWALK PER SDRSD G-07  
MATCH EXISTING COLOR

EXIST 6" HP STL GAS IN CASING  
EXIST 2" PX GAS IN CASING

EXIST 6" WATER

EXIST 6" WATER

"M" 535+20.40  
45.12' RT ECR

"M" 535+53.68  
40.00' RT END SAWCUT  
44.49' RT END C&G TYPE G PER SDRSD G-02  
44.99' RT END CONCRETE SIDEWALK PER SDRSD G-07

"C" 11+25.31, 57.97' RT  
BEG COLORED CONCRETE  
SIDEWALK PER SDRSD G-07  
MATCH EXISTING COLOR  
END TRUNCATED DOMES  
PER SDRSD G-30  
SEE SHEET 5

"C" 11+79.93, 57.80' LT  
END COLORED CONCRETE  
SIDEWALK PER SDRSD G-07  
MATCH EXISTING COLOR  
END TRUNCATED DOMES  
PER SDRSD G-30  
SEE SHEET 5

"C" 11+27.75, 69.16' RT  
BEG GALVANIZED STEEL  
RAILING PAINTED - BLUE  
(MATCH EXIST)

"C" 11+29.81, 83.43' RT  
BEG COLORED CONCRETE SIDEWALK PER  
SDRSD G-07 MATCH EXISTING COLOR

"C" 11+46.55, 69.16' RT  
AP GALVANIZED STEEL RAILING  
PAINTED - BLUE  
(MATCH EXIST)

"C" 11+49.08, 82.21' RT  
END COLORED CONCRETE SIDEWALK  
PER SDRSD G-07 MATCH EXISTING COLOR

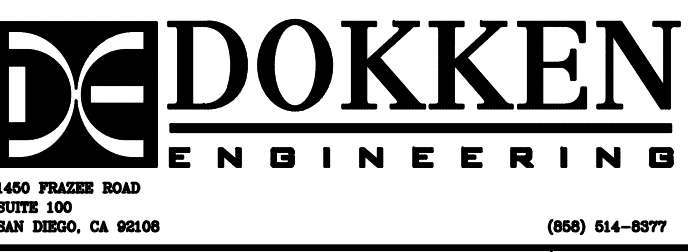
APN 381-052-07  
VESTAR KIMCO  
SANTEE L P

"C" 11+53.78, 72.61' RT  
END GALVANIZED STEEL  
RAILING PAINTED - BLUE  
(MATCH EXIST)

"C" 11+53.78, 72.61' RT  
END GALVANIZED STEEL  
RAILING PAINTED - BLUE  
(MATCH EXIST)

"C" 11+53.78, 72.61' RT  
END GALVANIZED STEEL  
RAILING PAINTED - BLUE  
(MATCH EXIST)

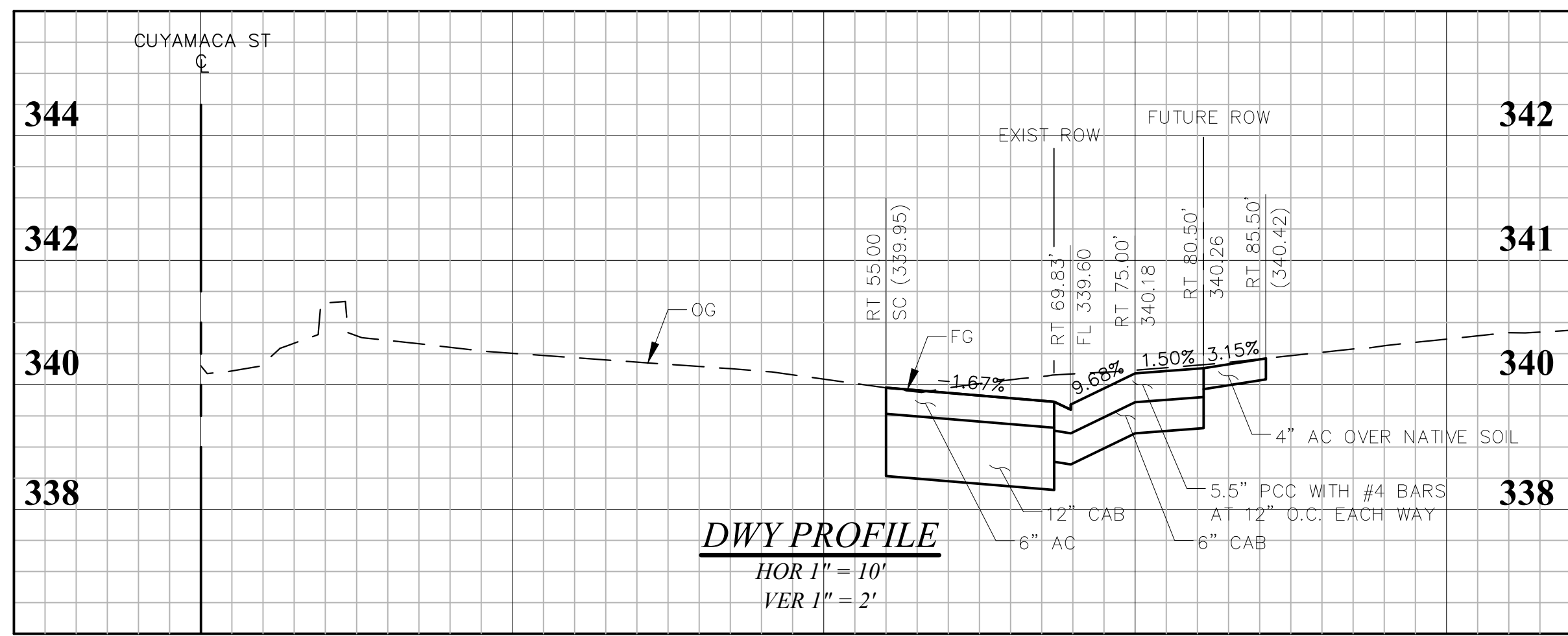
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END GALVANIZED STEEL  
RAILING PAINTED - BLUE  
(MATCH EXIST)



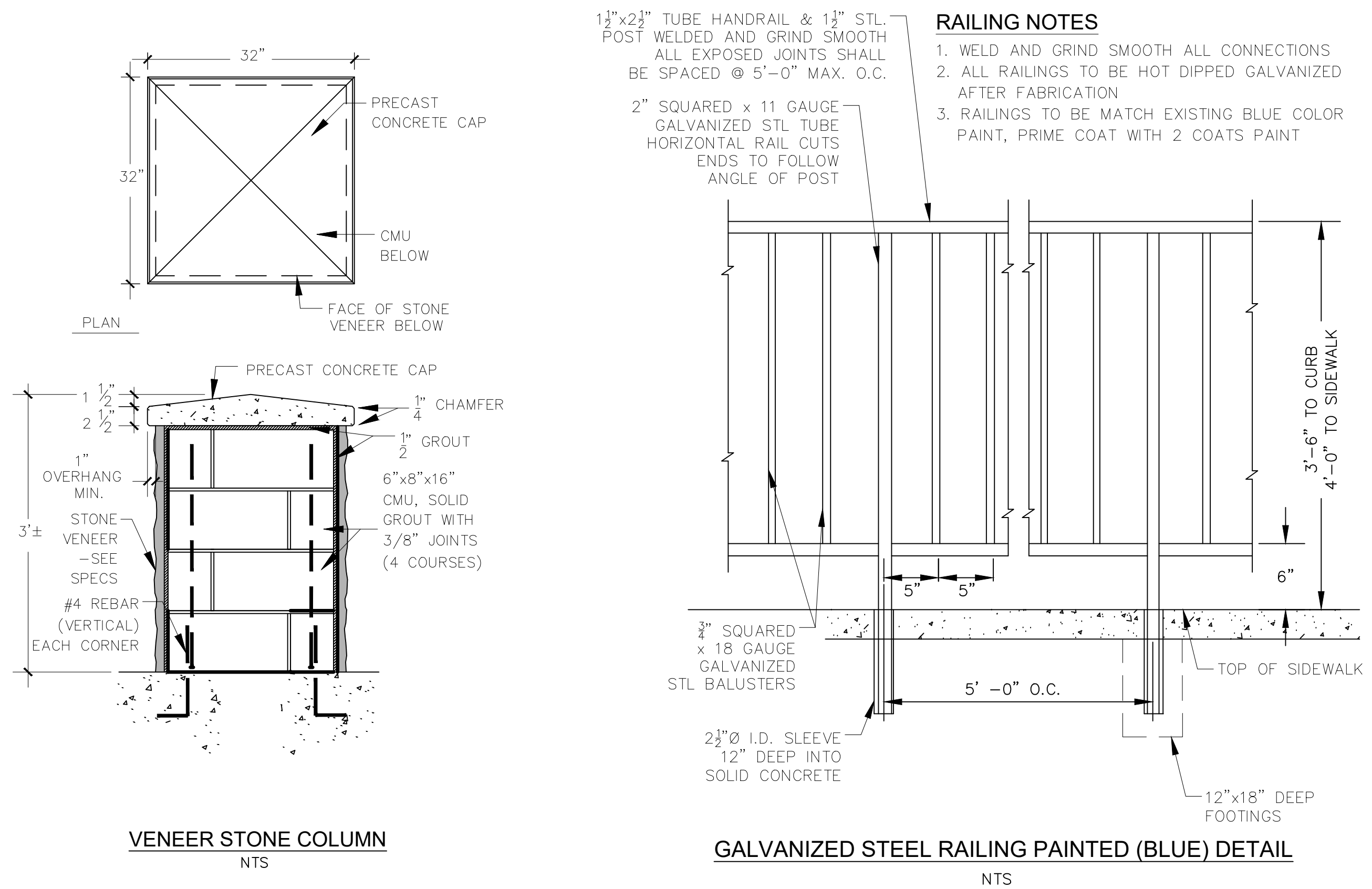
CONTRACTOR	CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK	SCALE	DESIGNED BY	DRAWN BY	CHECKED BY	REVIEWED	CITY OF SANTEE	DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO.	DRAWING NO.
INSPECTOR							STATION 2015 PER ROS 11302, LEAD PILES & BRIGGS TAG STAMPED "RICE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.	HORIZ: 1" = 20' VERT: 1" = 4'	VINCENT MALONG	VINCENT MALONG	MARK TARRALL		PLAN AND PROFILE		CIP 2014-02	2021-103
DATE COMPLETED							ELEVATION: 337.97, M.S.L. DATUM		PLANS PREPARED UNDER THE SUPERVISION OF	MARK TARRALL	DATE 01/12/2024	BY	CUYAMACA STREET RIGHT TURN POCKET			SHEET 4 OF 21
									RCE NO.	71953	EXPIRES 12/31/2025	PROJECT ENGINEER				

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024



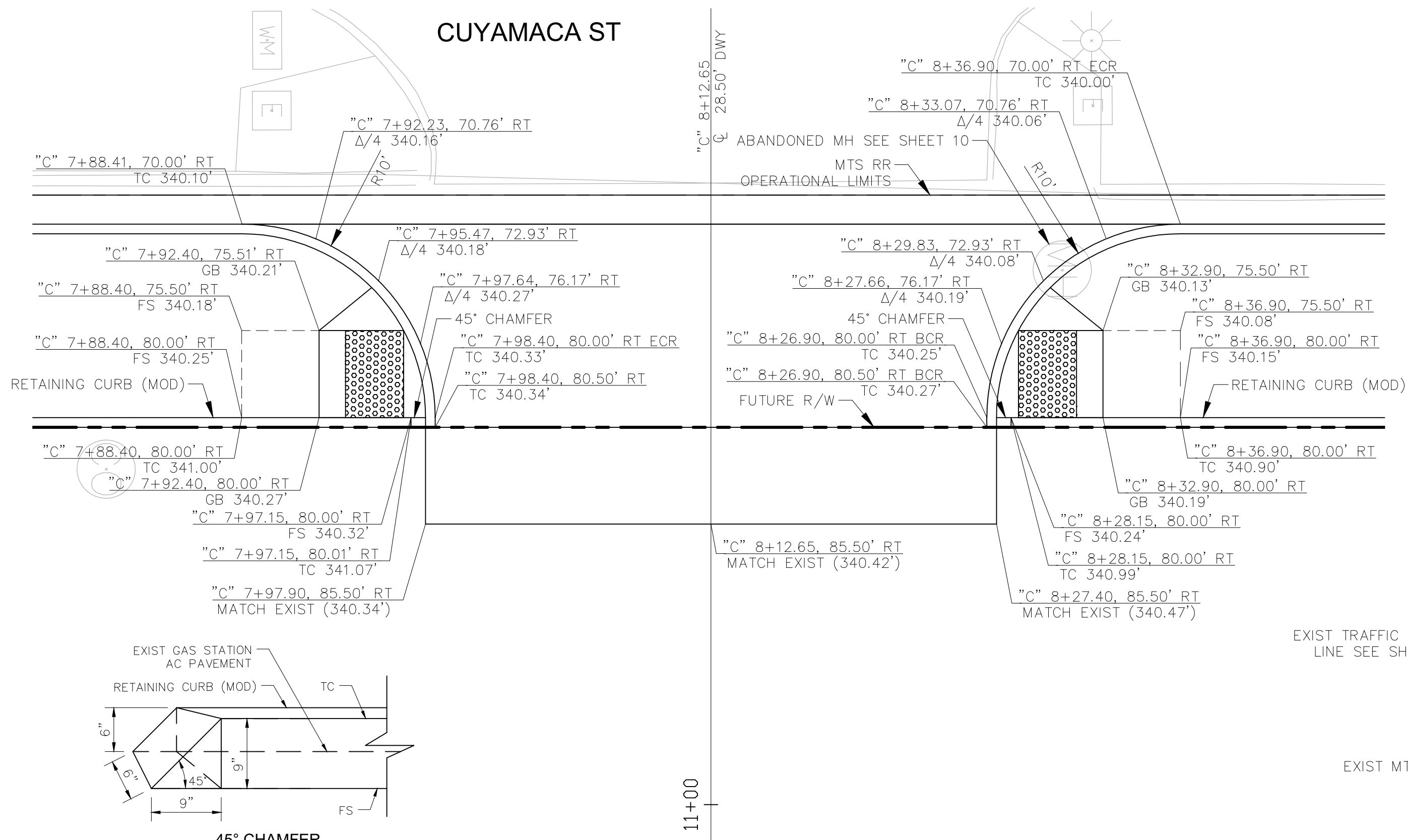


**DWY PROFILE**  
 HOR 1" = 10'  
 VER 1" = 2'



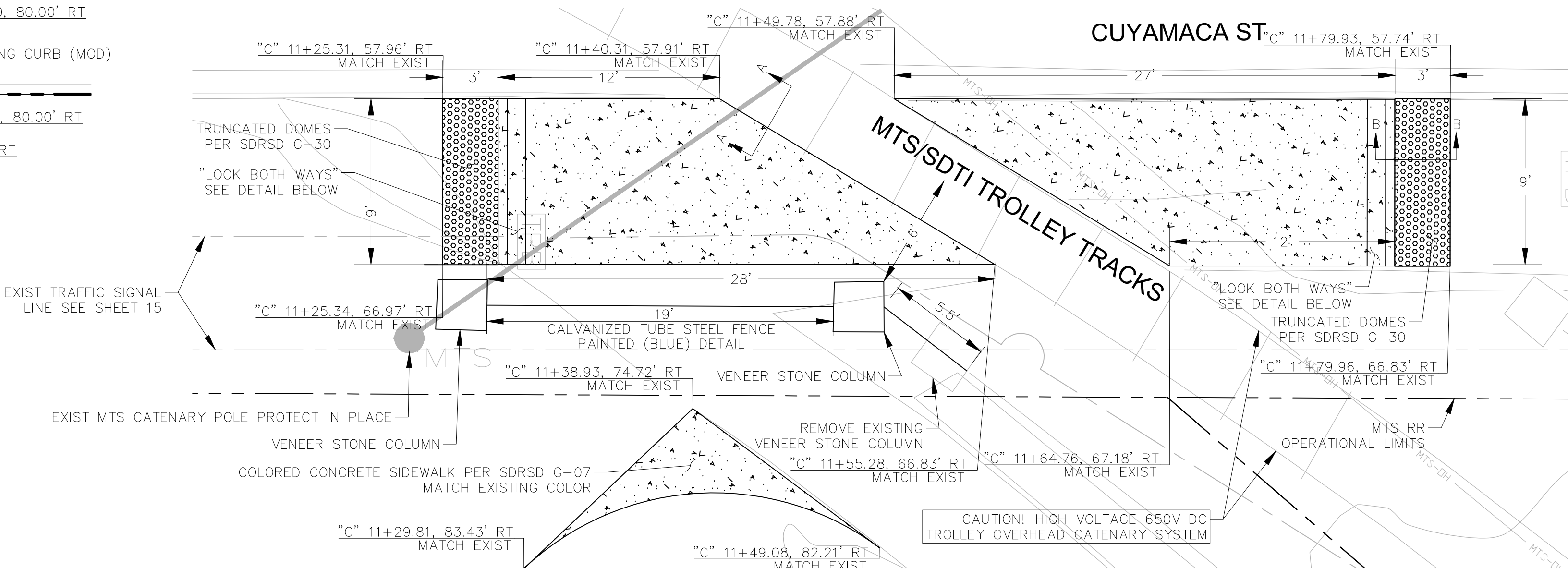
**VENEER STONE COLUMN**  
 NTS

**GALVANIZED STEEL RAILING PAINTED (BLUE) DETAIL**  
 NTS



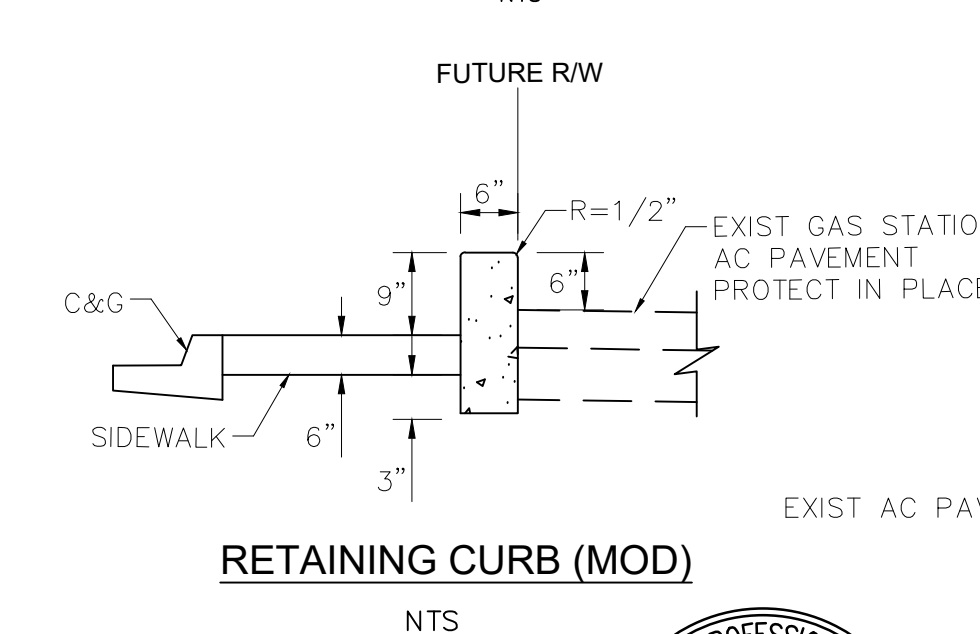
**DWY PLAN**  
 HOR 1" = 5'

**CONCRETE DRIVEWAY - PW-21A DETAIL**  
 AS NOTED



**PLAN**  
 HOR 1" = 5'

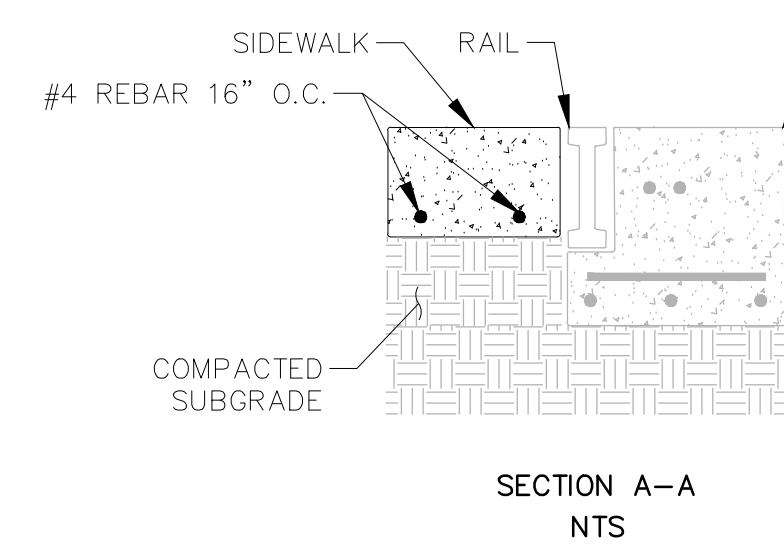
**MTS CROSSING DETAIL**  
 AS NOTED



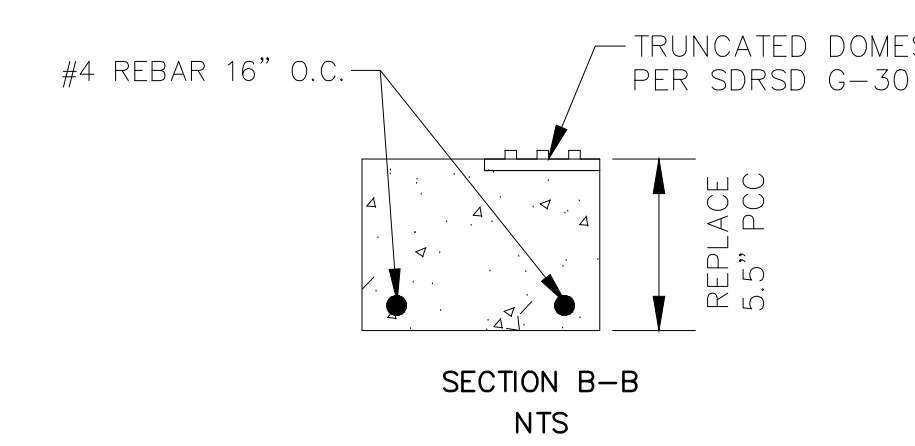
**RETAINING CURB (MOD)**  
 NTS

**COLD MILLING (2") AND 2" AC PAVEMENT**  
 NTS

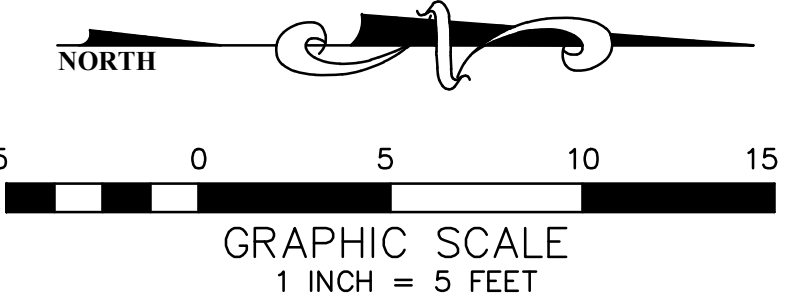
**"LOOK BOTH WAYS" DETAIL**  
 NTS



**SECTION A-A**  
 NTS



**SECTION B-B**  
 NTS

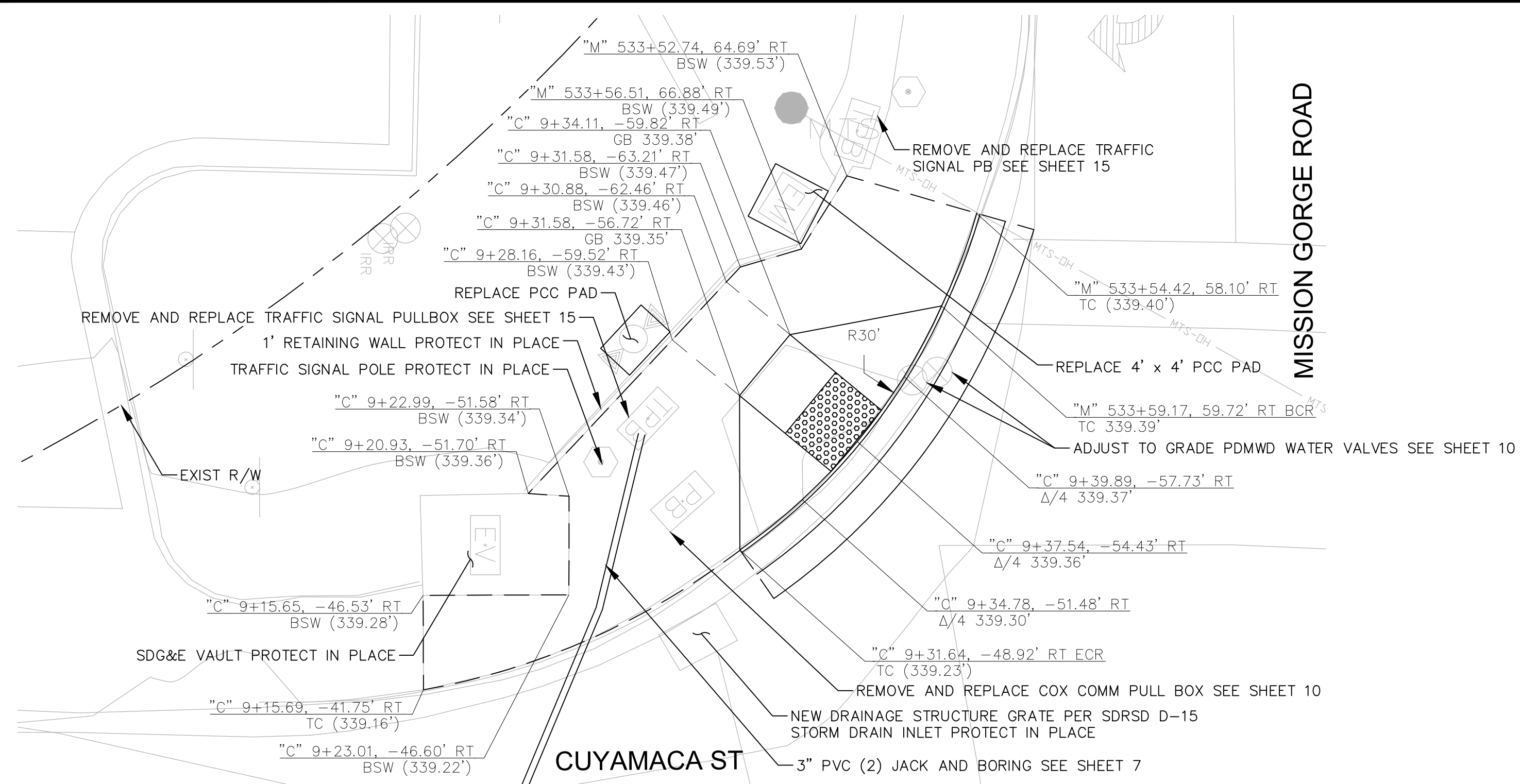


**GRAPHIC SCALE**  
 1 INCH = 5 FEET



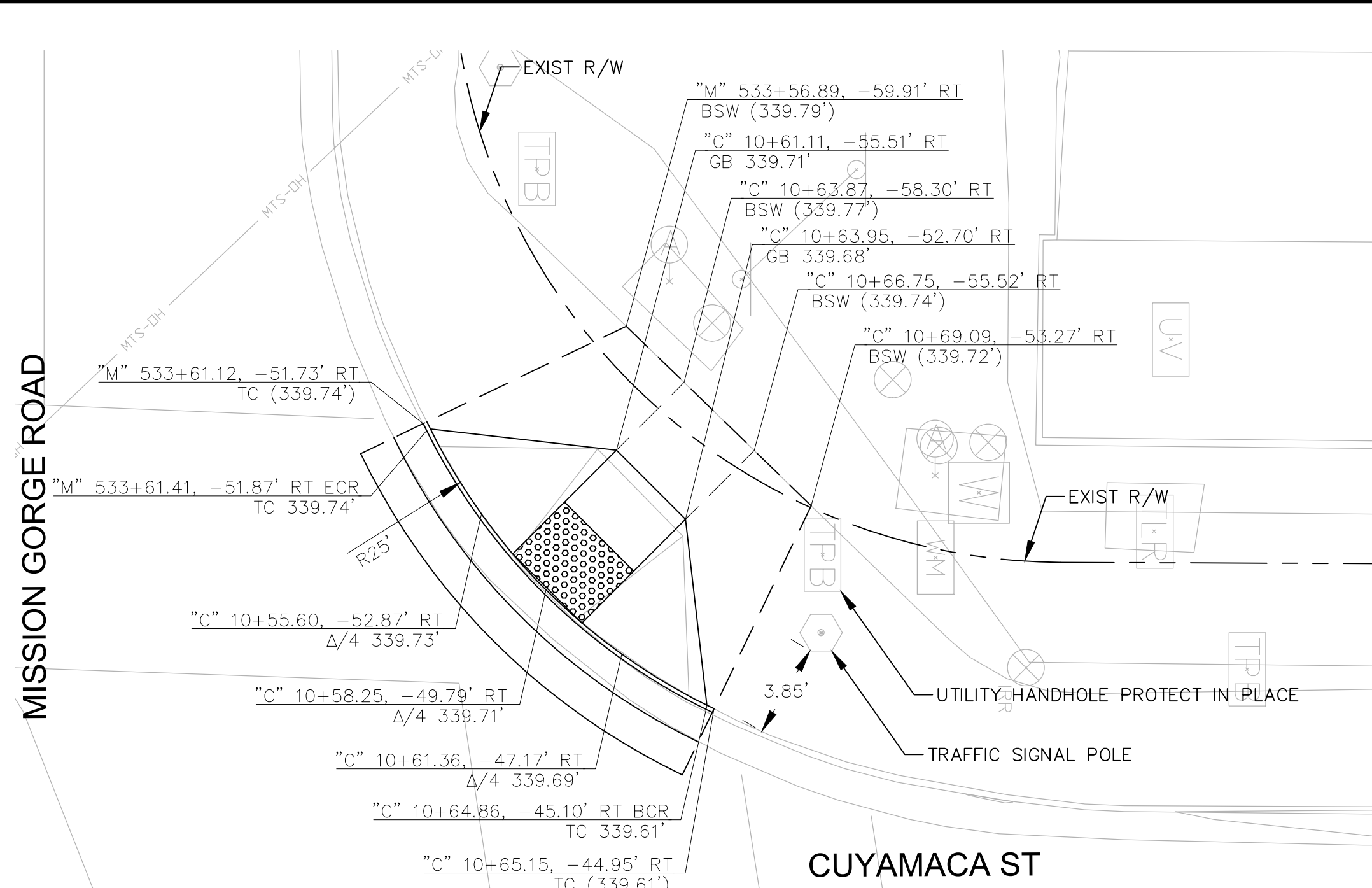
CONTRACTOR	INSPECTOR	DATE COMPLETED	CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK STATION 2015 PER ROS 11302 - LEAD PILING & BRASS TAG STAMPED "ICE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.	SCALE HORIZ: AS NOTED VERT: 1" = 2'	DESIGNED BY VINCENT MALONG	DRAWN BY VINCENT MALONG	CHECKED BY MARK TARRALL	REVIEWED	CITY OF SANTEE DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO. CIP 2014-02	DRAWING NO. 2021-104
									ELEVATION: 337.97, M.S.L. DATUM		PLANS PREPARED UNDER THE SUPERVISION OF MARK TARRALL	DATE 01/12/2024	EXPIRES 12/31/2025	PROJECT ENGINEER	CONSTRUCTION DETAIL CUYAMACA STREET RIGHT TURN POCKET		SHEET 5 OF 21

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024



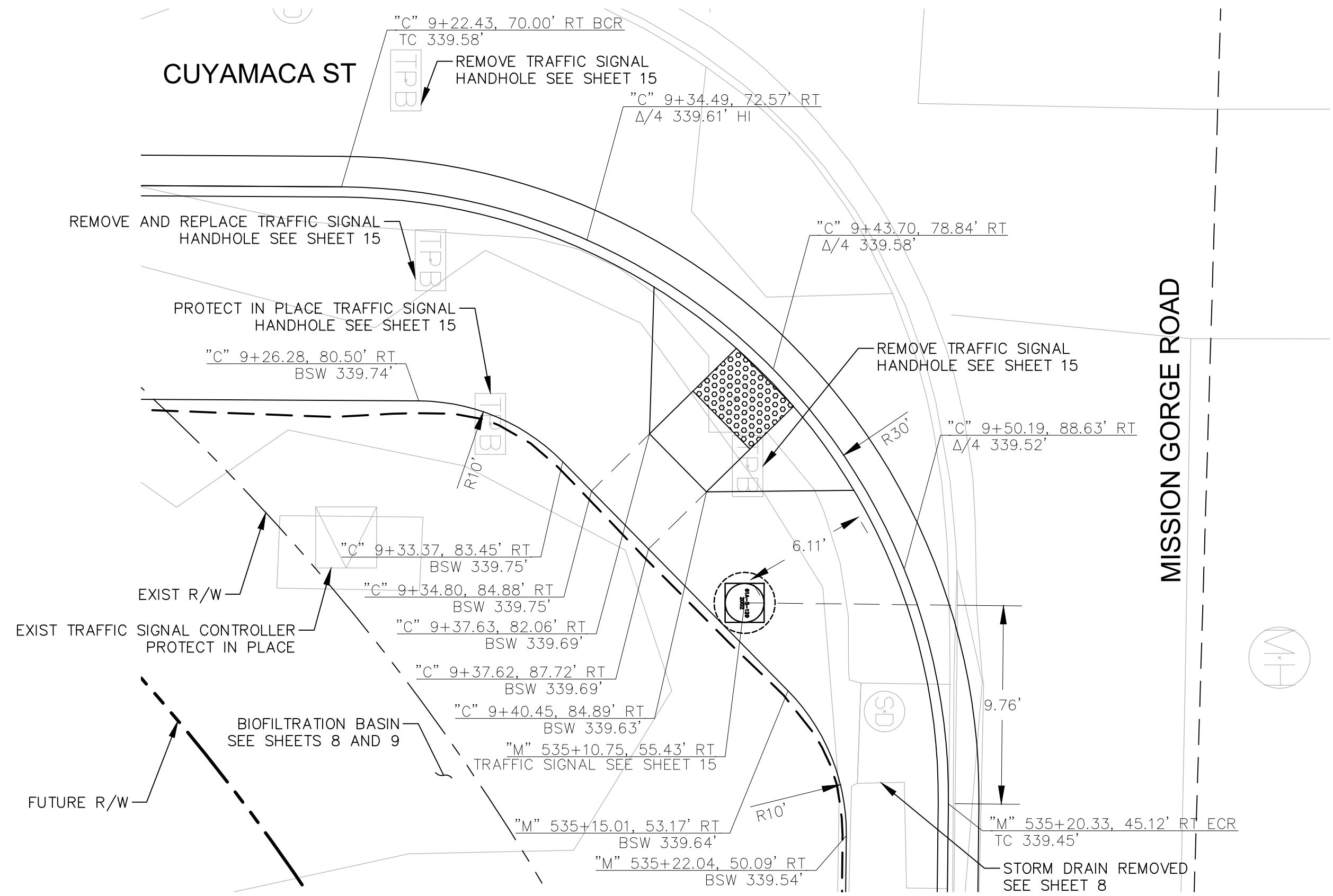
SW CURB RETURN  
CONCRETE CURB RAMP (TYPE A, PER G-27) DETAIL

HOR 1"=5'



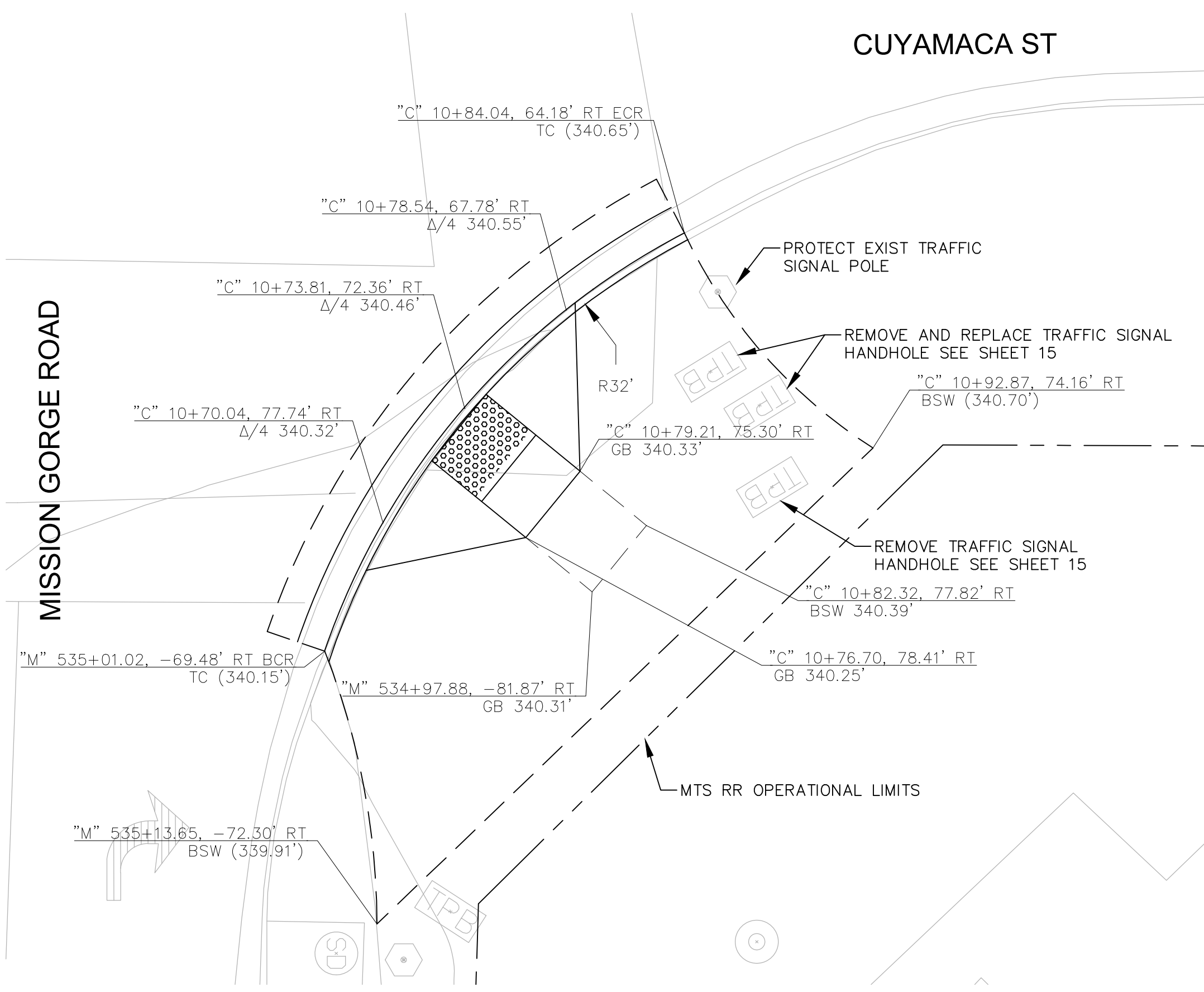
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CONCRETE CURB RAMP (TYPE A, PER G-27) DETAIL

HOR 1"=5'



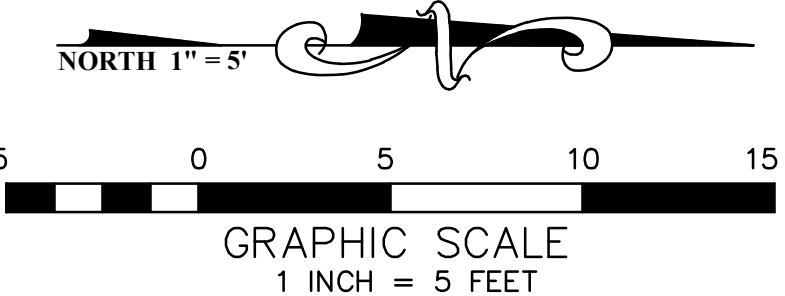
SE CURB RETURN  
CONCRETE CURB RAMP (TYPE A, PER G-27) DETAIL

HOR 1"=5'



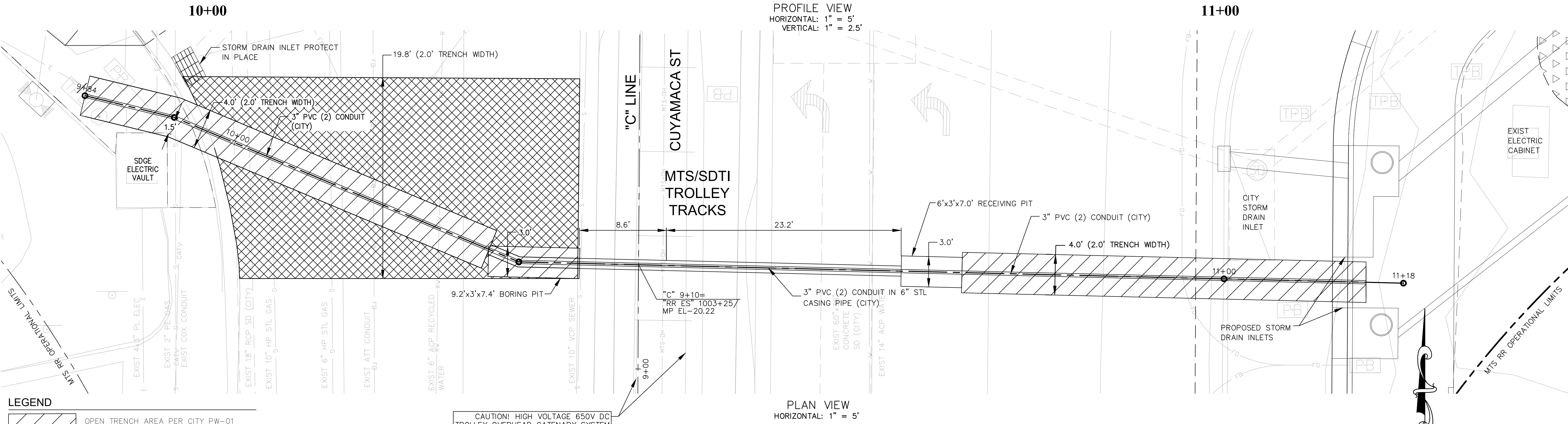
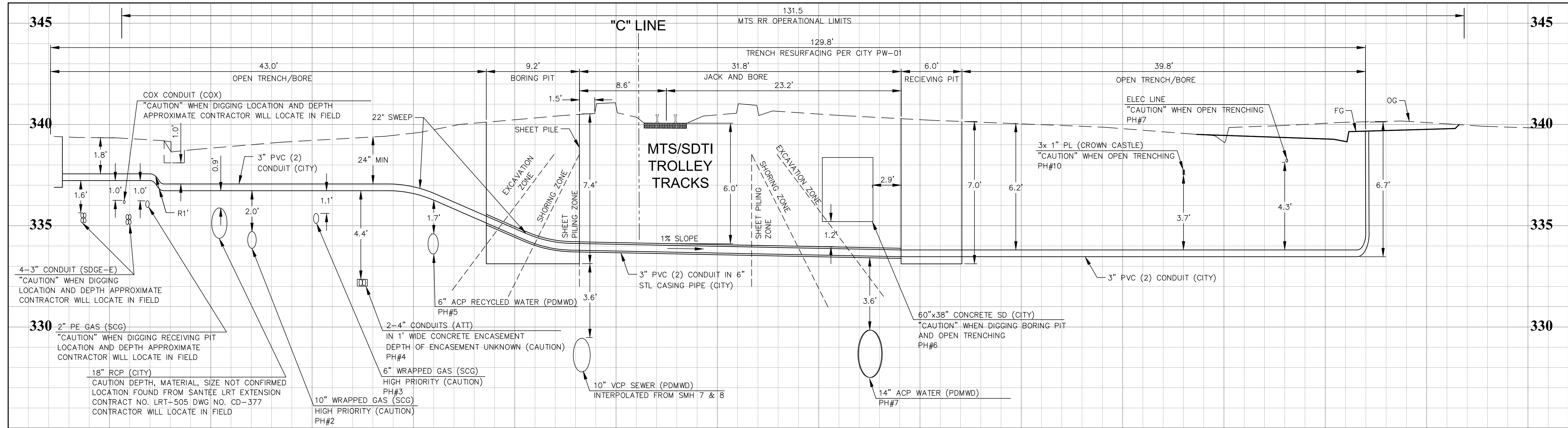
NE CURB RETURN  
CONCRETE CURB RAMP (TYPE A, PER G-27) DETAIL

HOR 1"=5'



CONTRACTOR	CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK STATION 2015 PER RCE 1132; LEAD PILE & BRASS TAG STAMPED "RCE 1693" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.	SCALE HORIZ: 1"=5' VERT: N/A	DESIGNED BY VINCENT MALONG	DRAWN BY VINCENT MALONG	CHECKED BY MARK TARRALL	REVIEWED	CITY OF SANTEE DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO. CIP 2014-02	DRAWING NO. 2021-105
INSPECTOR							ELEVATION: 337.97, M.S.L. DATUM	PLANS PREPARED UNDER THE SUPERVISION OF MARK TARRALL	DATE 01/12/2024	DATE 12/31/2025	PROJECT ENGINEER	CONSTRUCTION DETAIL CUYAMACA STREET RIGHT TURN POCKET		SHEET 6 OF 21	
DATE COMPLETED								RCE NO. 71953							

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024



**LEGEND**

	OPEN TRENCH AREA PER CITY PW-01
	COLD MILLING (2") AND 2" AC PAVEMENT

	CARRIER PIPE	CASING PIPE
CONTENTS TO BE HANDLED	ELECTRICAL/COMMUNICATION CONDUCTORS	3" PVC CONDUIT
OUTSIDE DIAMETER	3.5-INCHES	6.625-INCHES
PIPE MATERIAL	PVC PLASTIC, 7,450 PSI	STEEL, 45,000 PSI
SPECIFICATION AND GRADE	UL 651, SCHEDULE 40	ASTM A252, GRADE 3
WALL THICKNESS	0.216-INCHES	0.250-INCHES
ACTUAL WORKING PRESSURE	0 PSI	0 PSI
TYPE OF JOINT	GLUED	INTERLOCKING
COATING	NONE	ASPHALT
METHOD OF INSTALLATION	HAND PULLED	BORING

**SEALS:** BOTH ENDS:  ONE END:  TYPE: EPDM RUBBER WITH STAINLESS STEEL BANDS

BURY: BASE OF RAIL TO TOP OF CASING 6 FT. 0 IN.

BURY: (NOT BENEATH TRACKS) 2 FT. 0 IN.

BURY: (ROADWAY DITCHES) FT. IN.

TYPE, SIZE AND SPACING OF INSULATORS OR SUPPORTS: CENTERED, NON-RESTRAINING POLYETHYLENE, 5" OD, 4' SPACING

DISTANCE C.L. TRACK TO FACE OF JACKING/RECEIVING PITS 8 FT. 7 IN.

BURY: BASE OF RAIL TO BOTTOM JACKING/RECEIVING PITS 7 FT. 0 IN.

CATHODIC PROTECTION? YES  NO



CONTRACTOR	INSPECTOR	DATE COMPLETED

CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD

SCALE	DESIGNED BY	DRAWN BY	CHECKED BY
HORIZ: 1" = 2.5'	VINCENT MALONG	VINCENT MALONG	MARK TARRALL
VERT: 1" = 5'	PLANS PREPARED UNDER THE SUPERVISION OF MARK TARRALL DATE 01/12/2024 RCE NO. 71953 EXPIRES 12/31/2025		

REVIEWED	BY	PROJECT ENGINEER

**CITY OF SANTEE**  
 DEPARTMENT OF DEVELOPMENT SERVICES  
**JACK AND BORING DETAIL**  
**CUYAMACA STREET RIGHT TURN POCKET**

CITY W.O. NO.	DRAWING NO.
CIP 2014-02	2021-106

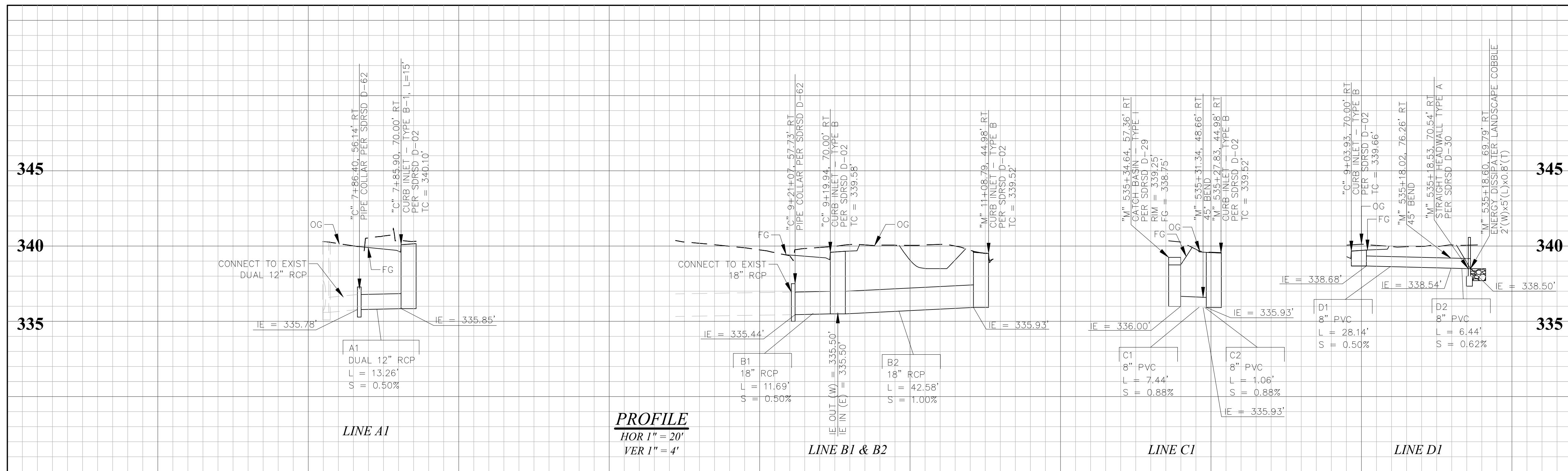
SHEET 7 OF 21

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024

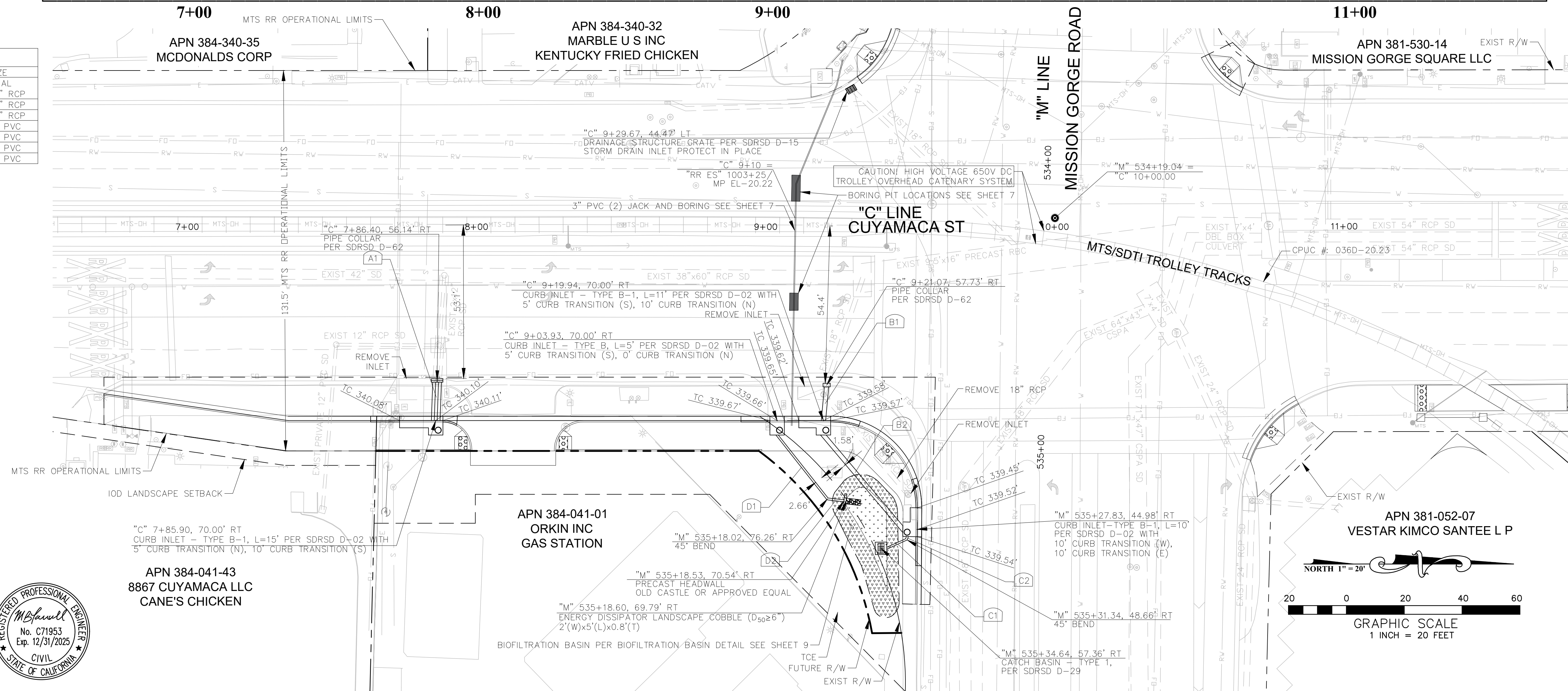


**GENERAL STORM DRAIN NOTES**

1. CONTRACTOR TO POTHOLE ALL CONNECTION STORM DRAINS TO VERIFY DEPTHS.
  2. ADJUST ALL MANHOLES TO NEW GRADE.
  3. CONFLICTING FRANCHISE UTILITIES TO BE RELOCATED BY OTHERS.
  4. ALL CURB TRANSITIONS FOR CURB INLETS ARE 10' UNLESS OTHERWISE NOTED.
  5. INCLUDE TRASH CAPTURE DEVICES/INLET FILTERS FOR ALL NEW STORM DRAIN INLETS AND CATCH BASIN.
- \* APPLIES TO ALL STORM DRAIN SHEETS



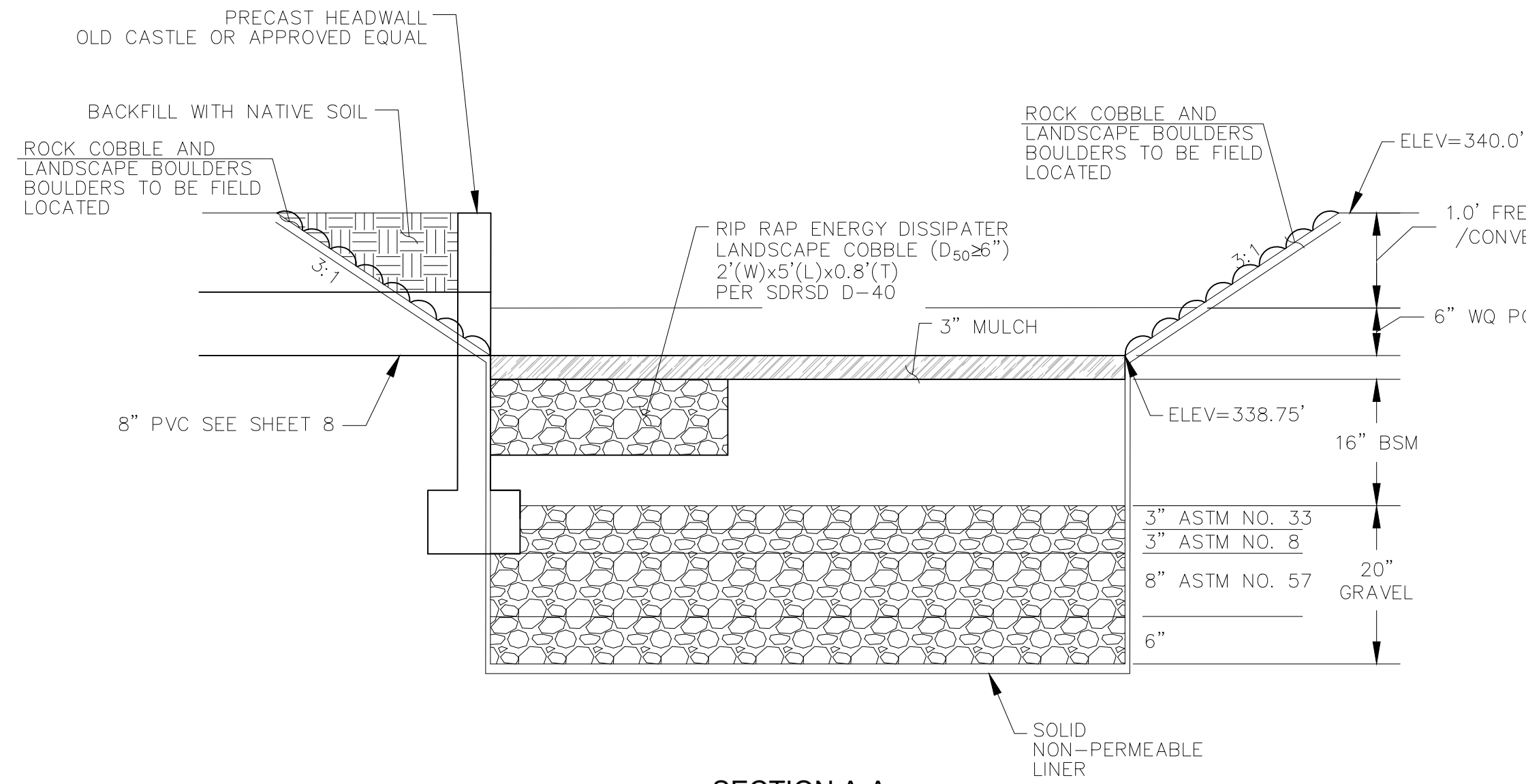
NO.	BEARING/DELTA	RADIUS	LENGTH	SIZE
A1	N 87°31'53" W	-	13.26'	DUAL 12" RCP
B1	S 84°26'07" E	-	11.69'	18" RCP
B2	N 50°10'12" E	-	42.58'	18" RCP
C1	N 24°11'25" W	-	7.44'	8" PVC
C2	N 69°11'25" W	-	1.06'	8" PVC
D1	N 52°32'52" E	-	28.14'	8" PVC
D2	N 06°47'25" E	-	6.44'	8" PVC



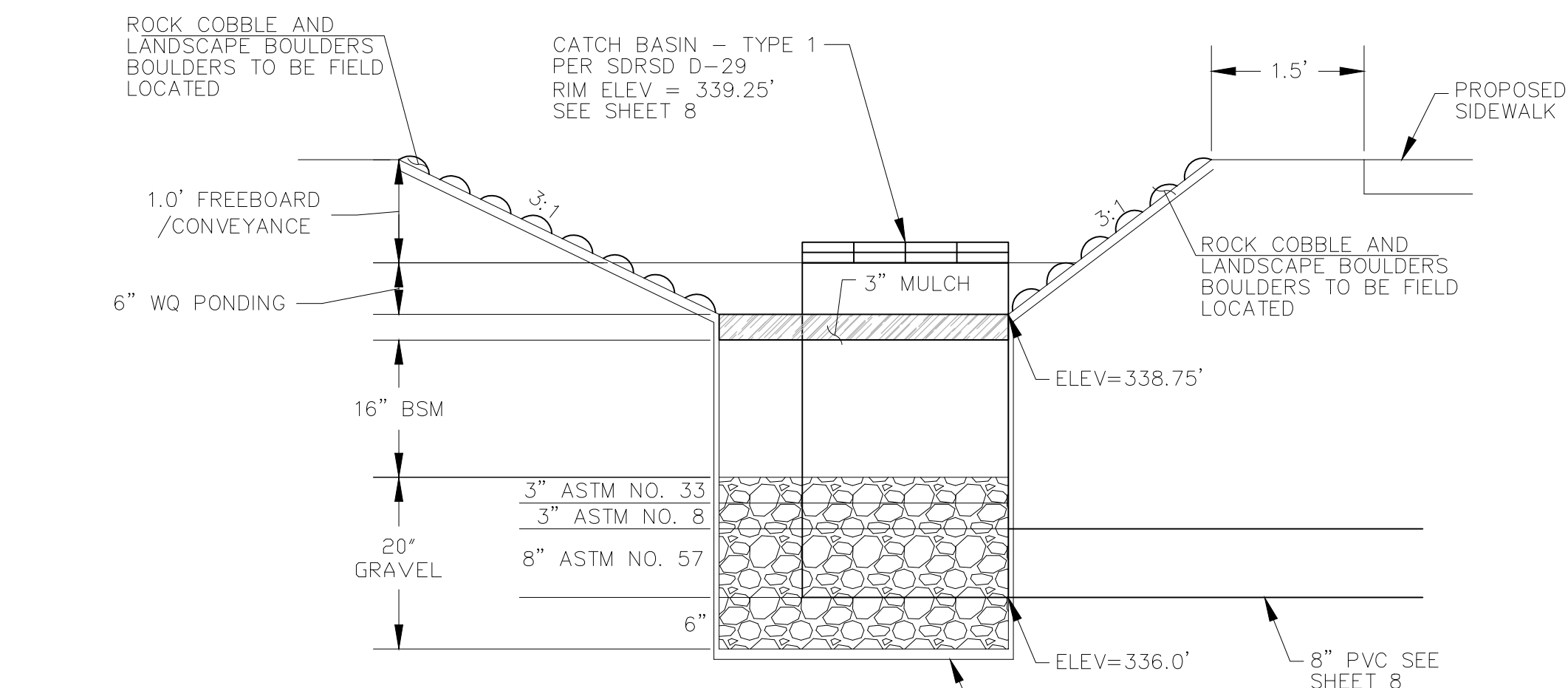
CONTRACTOR	CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK	SCALE	DESIGNED BY	DRAWN BY	CHECKED BY	REVIEWED	CITY OF SANTEE	DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO.	DRAWING NO.
INSPECTOR							STATION 2015 PER RGS 11292 - 1640 PLUS 8 BRASS TAG STAMPED "RCE 1693" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.	HORIZ: 1" = 20' VERT: 1" = 4'	VINCENT MALONG	VINCENT MALONG	MARK TARRALL		DRAINAGE PLAN AND PROFILE		CIP 2014-02	2021-107
DATE COMPLETED							ELEVATION: 337.97, M.S.L. DATUM		PLANS PREPARED UNDER THE SUPERVISION OF	MARK TARRALL	DATE 01/12/2024	BY	CUYAMACA STREET RIGHT TURN POCKET			SHEET 8 OF 21
									RCE NO. 71953		EXPIRES 12/31/2025	PROJECT ENGINEER				

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024

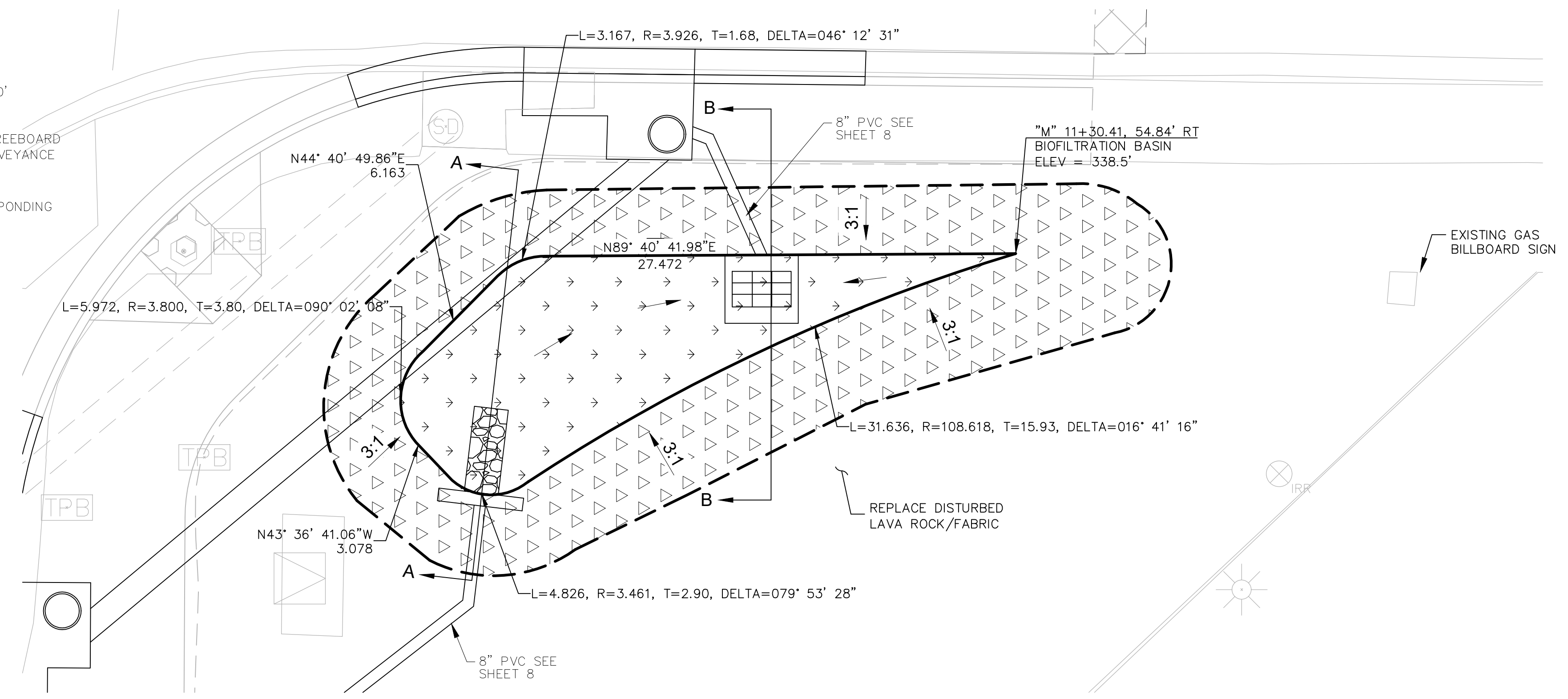




**SECTION A-A**  
NOT TO SCALE



**SECTION B-B**  
NOT TO SCALE



**BIOFILTRATION BASIN DETAIL**  
NOT TO SCALE



1400 PRAIRIE ROAD  
SUITE 100  
SAN DIEGO, CA 92108  
(619) 514-8777

CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD

STATION 2075 PER R08 11252 - LEAD PLUG & BRASS TAG STAMPED "RCE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.  
ELEVATION: 337.97, M.S.L. DATUM

SCALE	DESIGNED BY	DRAWN BY	CHECKED BY
HORIZ: N/A	VINCENT MALONG	VINCENT MALONG	MARK TARRALL
VERT: N/A	PLANS PREPARED UNDER THE SUPERVISION OF MARK TARRALL		
	RCE NO. 71953	DATE 01/12/2024	EXPIRES 12/31/2025

REVIEWED \_\_\_\_\_  
BY \_\_\_\_\_ PROJECT ENGINEER

CITY OF SANTEE DEPARTMENT OF DEVELOPMENT SERVICES  
DRAINAGE DETAIL  
CUYAMACA STREET RIGHT TURN POCKET

CITY W.O. NO. CIP 2014-02  
DRAWING NO. 2021-108  
SHEET 9 OF 21

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024



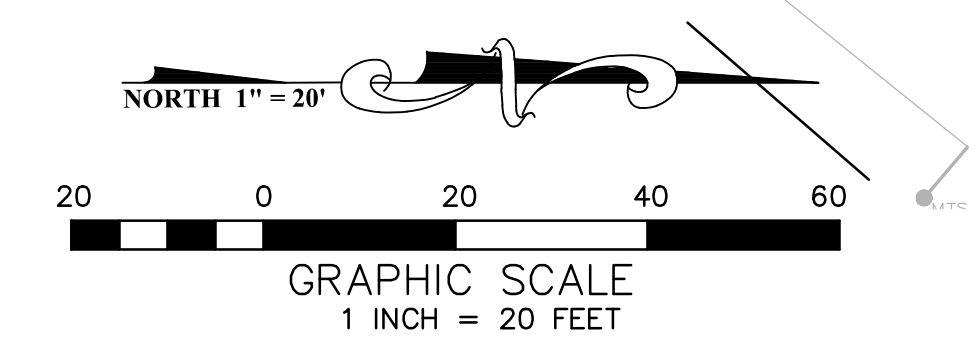
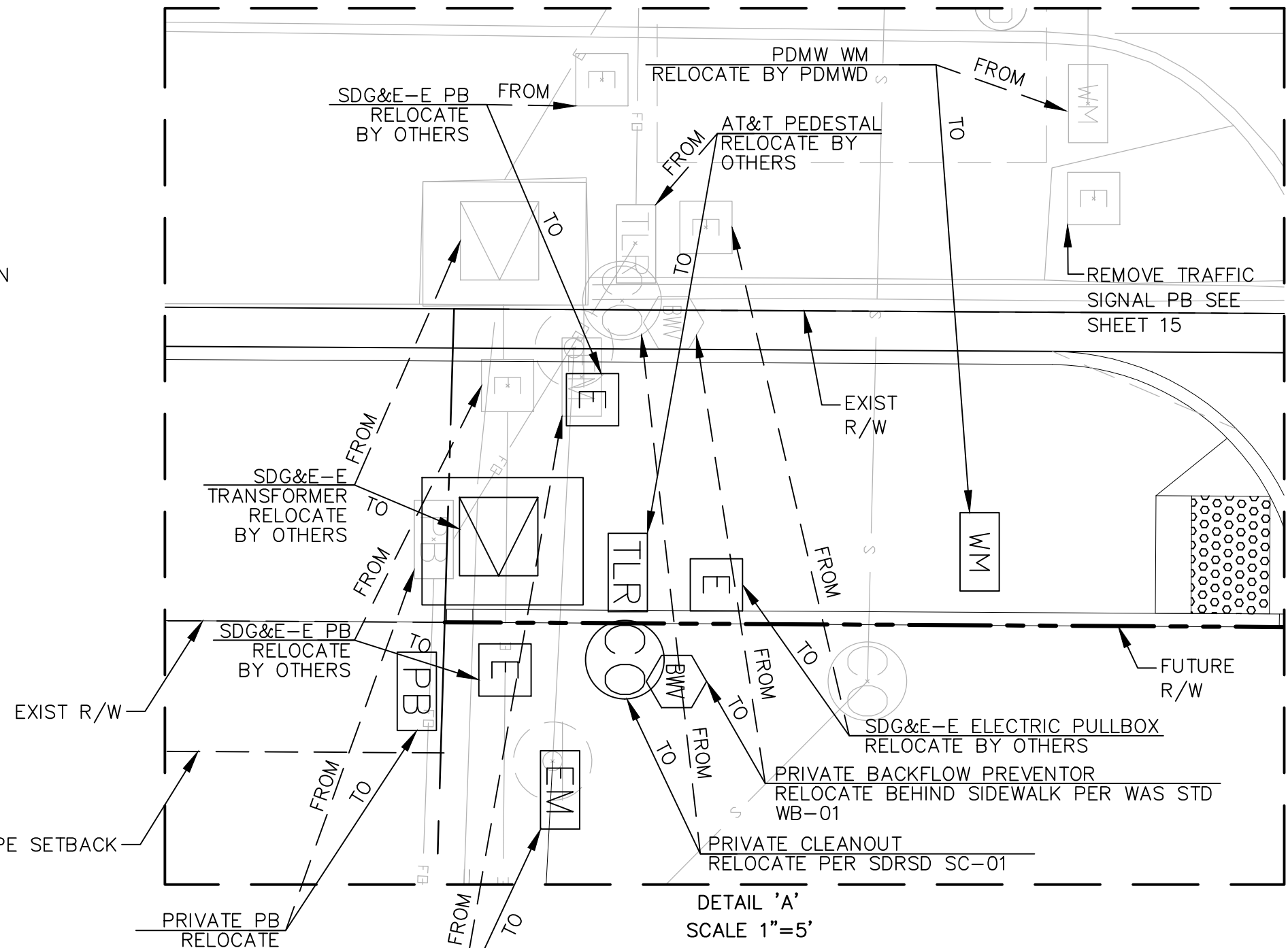
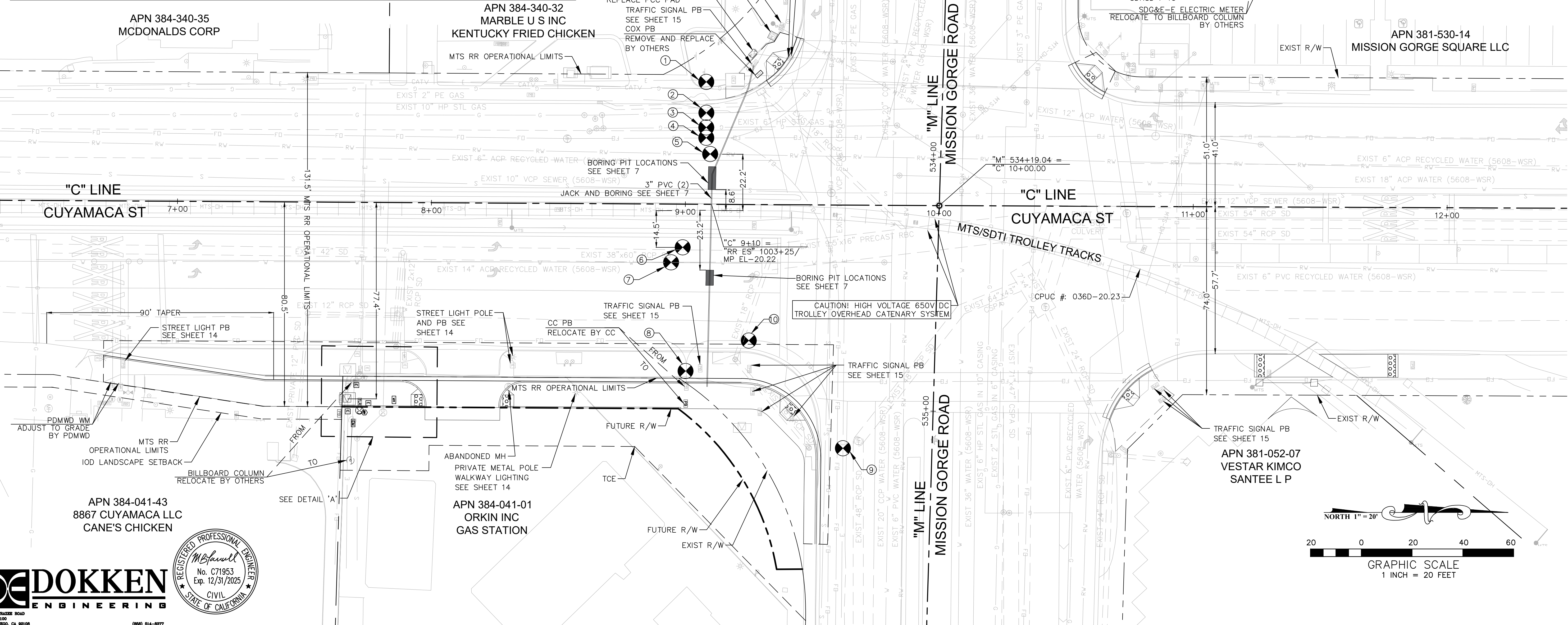
**LEGEND**

- FO — FIBER OPTIC — ATT DISTRIBUTION (ATT)
- FO — FIBER OPTIC — LEVEL 3 COMMUNICATION (LV3)
- FO — FIBER OPTIC — CROWN CASTLE (CC)
- W — WATER — PADRE DAM MUNICIPAL WATER DISTRICT (PDMWD)
- S — SEWER — PADRE DAM MUNICIPAL WATER DISTRICT (PDMWD)
- RW — RECYCLED WATER — PADRE DAM MUNICIPAL WATER DISTRICT (PDMWD)
- G — GAS — SDG&E GAS (SDGE-G)
- E — ELECTRIC — SDG&E ELECTRIC (SDGE-E)
- CATV — CATV — COX COMMUNICATIONS (COX)
- SD — STORM DRAIN — CITY OF SANTEE (CITY)

**GENERAL NOTES**

1. SHUT DOWN OF MAINS TO BE DONE BY PADRE DAM MUNICIPAL WATER DISTRICT.
2. THE LOCATION OF EXISTING DRY UTILITIES ARE BASED UPON UTILITY MAPS, AS-BUILTS AND FIELD OBSERVATIONS.
3. CONTRACTOR IS RESPONSIBLE TO POTHOLE IN AREAS WHERE EXACT LOCATION OR DEPTH OF FACILITIES NEED TO BE KNOWN.
4. BEFORE ANY EXCAVATION WORK, CALL USA ALERT AT 811 AT LEAST TWO DAYS PRIOR TO DIGGING.
5. THE LOCATION OF THE UTILITIES SHOWN ON THIS SHEET ARE FOR REFERENCE PURPOSES ONLY AND BASED UPON UTILITY INFORMATION, WORK ORDERS OR UTILITY DESIGN ANTICIPATED REQUIREMENTS FOR EACH OF THE UTILITIES. NO UTILITIES SHOULD BE INSTALLED BASED ON THIS DRAWING, CONTRACTOR SHOULD REFER TO EACH UTILITIES WORK ORDER DESIGN FOR THE INSTALLATION OF THEIR SYSTEMS.
6. ASSUME ALL UTILITIES ARE PROTECT IN PLACE UNLESS OTHERWISE NOTED.

NO	UTILITY	NORTHING	EASTING	STATION/OFFSET	TOP OF PIPE DEPTH	SURFACE ELEVATION
1	SDG&E ELECTRIC	1885631.4057	6336228.9657	"C" 9+07.98, 48.16' LT	3.74'	339.64'
2	SDG&E GAS	1885631.4153	6336241.1249	"C" 9+08.06, 35.93' LT	4.30'	339.02'
3	SDG&E GAS	1885631.3916	6336246.8768	"C" 9+08.07, 30.25' LT	3.62'	339.20'
4	AT&T FIBER OPTIC	1885631.4081	6336250.9512	"C" 9+08.11, 26.10' LT	7.00'	339.32'
5	PDMWD RECYCLED WATER	1885632.9210	6336257.3640	"C" 9+08.14, 19.85' LT	5.14'	339.74'
6	CITY STORM DRAIN	1885622.0843	6336294.0743	"C" 8+99.03, 17.00' RT	1.96'	340.55'
7	PDMWD WATER	1885617.5170	6336300.0004	"C" 8+94.50, 22.95' RT	10.50'	340.36'
8	CITY STREET LIGHT	1885623.2132	6336342.7856	"C" 9+00.44, 65.63' RT	1.82'	340.00'
9	SDG&E ELECTRIC	1885685.2001	6336373.1723	"M" 10+96.62, 35.02' RT	1.74'	339.64'
10	CC FIBER OPTIC	1885648.1326	6336330.7860	"C" 9+25.29, 53.61' RT	1.80'	340.00'



**DOKKEN ENGINEERING**  
 1400 PRAIRIE ROAD  
 SUITE 100  
 SAN DIEGO, CA 92108  
 (619) 514-8777

REGISTERED PROFESSIONAL ENGINEER  
 No. C71953  
 Exp. 12/31/2025  
 CIVIL  
 STATE OF CALIFORNIA

CONTRACTOR	INSPECTOR	DATE COMPLETED	CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK	STATION 2015 PER RDS 11392 - LEAD PILE & BRASS TAG STAMPED "RCE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.	SCALE	DESIGNED BY	DRAWN BY	CHECKED BY	REVIEWED	CITY OF SANTEE	DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO.	DRAWING NO.
									ELEVATION: 337.97, M.S.L. DATUM	HORIZ: 1"=20'	VINCENT MALONG	VINCENT MALONG	MARK TARRALL			UTILITY PLAN		CIP 2014-02	2021-109
										VERT: N/A	PLANS PREPARED UNDER THE SUPERVISION OF	MARK TARRALL	DATE	01/12/2024	BY	CUYAMACA STREET RIGHT TURN POCKET			SHEET 10 OF 21
											RCE NO.	71953	DATE	12/31/2025	PROJECT ENGINEER				

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024



**LEGEND**

- CONSTRUCTION SIGN (AS NOTED ON PLAN)
- TYPE II BARRICADE
- TYPE III BARRICADE
- TEMPORARY PLASTIC DRUM (TPD)
- DIRECTION OF TRAVEL
- PAVEMENT MARKING - PAINT (AS NOTED ON PLANS)
- LANE STRIPING - PAINT (AS NOTED ON PLANS)
- NTS - LENGTH SHOWN IS NOT DRAWN TO SCALE
- FLASHING ARROW BOARD
- CONSTRUCTION LINE WIDTH
- POST MOUNTED SIGN
- WORK AREA
- MTS FLAGPERSON

**TRAFFIC SIGN LEGEND:**

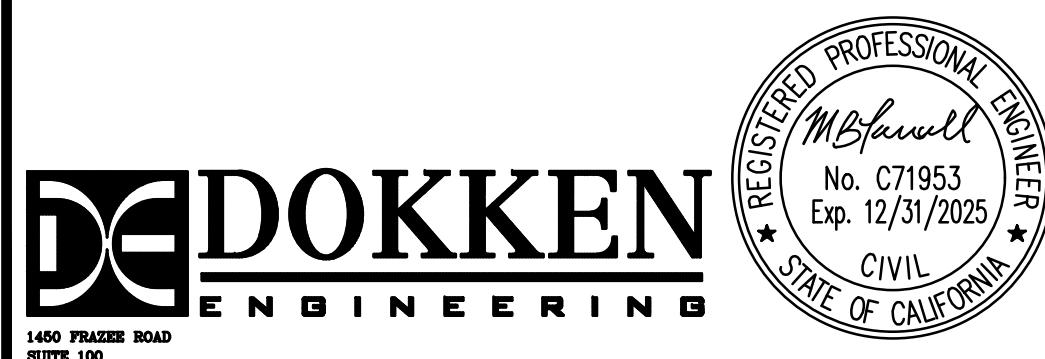
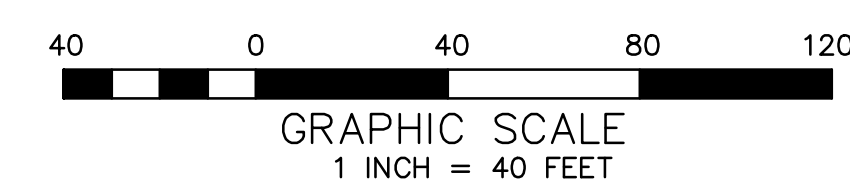
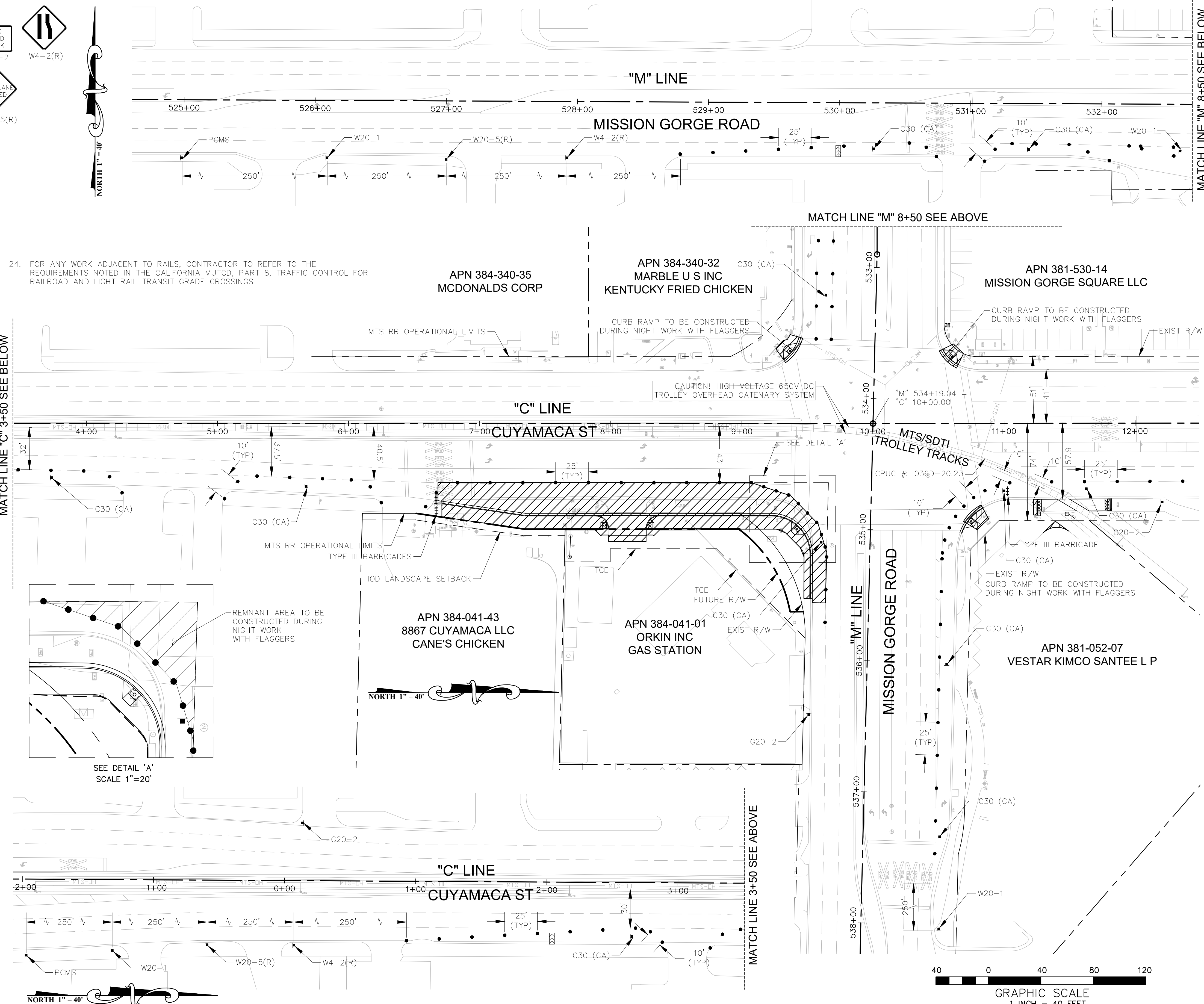
- LANE CLOSED  
C30(CA)
- END ROAD WORK  
G20-2
- RIGHT LANE CLOSED  
W20-5(R)
- W4-2(R)
- ROAD WORK AHEAD  
W20-1



**CONSTRUCTION NOTES**

1. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE CALIFORNIA MUTCD, (MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES) LATEST EDITION.
2. THE CITY ENGINEER OR HIS REPRESENTATIVE HAS THE AUTHORITY TO INITIATE FIELD CHANGES TO ASSURE PUBLIC SAFETY.
3. ALL TRAFFIC CONTROL DEVICES SHALL BE REMOVED FROM VIEW WHEN NOT IN USE.
4. WORK HOURS SHALL BE AS WRITTEN IN THE CONTRACT SPECIFICATIONS UNLESS APPROVED OTHERWISE BY THE CITY ENGINEER.
5. TRENCHES MUST BE BACK FILLED OR PLATED DURING NON-WORKING HOURS.
6. PEDESTRIAN CONTROLS SHALL BE PROVIDED AS REQUIRED BY THE CITY.
7. TEMPORARY "NO PARKING" SIGNS WILL BE POSTED 48 HOURS PRIOR TO COMMENCING WORK FOR THAT PHASE.
8. ACCESS TO ALL DRIVEWAYS AND INTERSECTING STREETS SHALL BE MAINTAINED AT ALL TIMES. POSITION MARKERS TO ALLOW A 25' TURN RADIUS IN AND OUT OF DRIVEWAYS AND STREETS.
9. ALL STRIPING REMOVED OR DAMAGED WILL BE REPLACED BY THE CONTRACTOR WITHIN 24 HOURS.
10. ALL WORKERS SHALL BE EQUIPPED WITH ORANGE VEST (OR A REFLECTIVE VEST AT NIGHT). ALL FLAGGERS SHALL BE EQUIPPED WITH A HARD, C28 "STOP/SLOW" PADDLE AND SHALL BE TRAINED IN THE PROPER FUNDAMENTALS OF FLAGGING TRAFFIC.
11. ANY WORK THAT DISTURBS NORMAL TRAFFIC SIGNAL OPERATIONS SHALL BE COORDINATED WITH THE CITY TRAFFIC ENGINEER 48 HOURS PRIOR TO BEGINNING CONSTRUCTION.
12. THE CONTRACTOR SHALL MAINTAIN ALL TRAFFIC CONTROL DEVICES 24 HOURS PER DAY AND 7 DAYS PER WEEK.
13. LANE CLOSURES, ROAD DETOURS, ROAD CLOSURES AND TRAFFIC SIGNAL MODIFICATIONS ASSOCIATED WITH NIGHT CONSTRUCTION ACTIVITIES WILL REQUIRE WARNING SIGNS TO BE PLACED AT LEAST ONE WEEK IN ADVANCE.
14. ALL CONSTRUCTION SIGNS SHALL BE INSTALLED ON TYPE II BARRICADES OR ON 4" X 4" WOOD POSTS IN THE GROUND.
15. CHANGEABLE MESSAGE BOARDS SHALL BE PROVIDED AT THE LOCATIONS PER THE CITY ENGINEER'S INSTRUCTIONS.
16. PROJECT INFORMATION SIGNS SHALL BE INSTALLED AT THE LOCATIONS PER THE CITY OF ENGINEER'S INSTRUCTIONS.
17. EXCAVATIONS ADJACENT TO TRAVEL LANES SHALL BE BACKFILLED WITHIN 2 WEEKS OF THE EXCAVATION UNLESS APPROVED OTHERWISE BY THE CITY ENGINEER.
18. TEMPORARY PAINTED STRIPING AND SIGNAGE MUST BE USED WHEN TRAFFIC PATTERNS ARE CHANGED. ALL CONFLICTING STRIPING AND SIGNS SHALL BE REMOVED PRIOR TO THE CHANGE IN THE TRAFFIC PATTERNS.
19. SIGNS SHOWN ARE MINIMUM REQUIRED. ADDITIONAL SIGNS SHALL BE INSTALLED AS DIRECTED BY THE CITY TRAFFIC ENGINEER OR AS REQUIRED.
20. CONTRACTOR SHALL SUBMIT TRAFFIC CONTROL PLANS TO THE CITY 10 WORKING DAYS IN ADVANCE FOR REVIEW AND APPROVAL FOR ITEMS OF WORK NOT COVERED BY THESE TRAFFIC CONTROL PLANS.
21. THE PHASING SHOWN ON THESE PLANS MAY BE MODIFIED BY THE CONTRACTOR. IF THE PHASING IS CHANGED, THE CONTRACTOR SHALL SUBMIT THEIR OWN PHASING/TRAFFIC PLANS TO THE CITY 10 WORKING DAYS IN ADVANCE FOR REVIEW AND APPROVAL PRIOR TO THE START OF CONSTRUCTION.
22. ALL WORK REQUIRING LANE CLOSURES NIGHT WORK 9:00 PM TO 5:00 AM.
23. MTS RIGHT OF ENTRY PERMIT REQUIRED FOR ALL WORK, TRAFFIC CONTROL, EQUIPMENT LOCATED WITHIN 15' IF OUTER TRACK.

MATCH LINE "C" 3+50 SEE BELOW



CONTRACTOR	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK STATION 2015 PER RGS 11322 - LEAD PLUG & BRASS TAG STAMPED "RCE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.	SCALE HORIZ: AS NOTED VERT: N/A	DESIGNED BY VINCENT MALONG	DRAWN BY VINCENT MALONG	CHECKED BY MARK TARRALL	REVIEWED	CITY OF SANTEE DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO. CIP 2014-02	DRAWING NO. 2021-110
INSPECTOR						ELEVATION: 337.97, M.S.L. DATUM		PLANS PREPARED UNDER THE SUPERVISION OF MARK TARRALL		DATE 01/12/2024	PROJECT ENGINEER	TRAFFIC HANDLING CUYAMACA STREET RIGHT TURN POCKET		SHEET 11 OF 21
DATE COMPLETED								RCE NO. 71953		EXPIRES 12/31/2025				

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024



**LEGEND**

- CONSTRUCTION SIGN (AS NOTED ON PLAN)
- TYPE II BARRICADE
- TYPE III BARRICADE
- TEMPORARY PLASTIC DRUM (TPD)
- DIRECTION OF TRAVEL
- PAVEMENT MARKING - PAINT (AS NOTED ON PLANS)

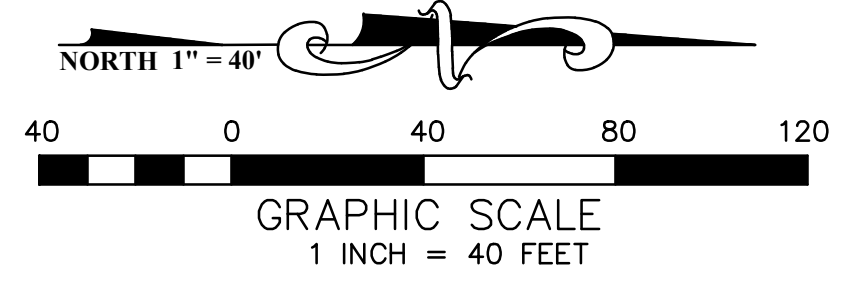
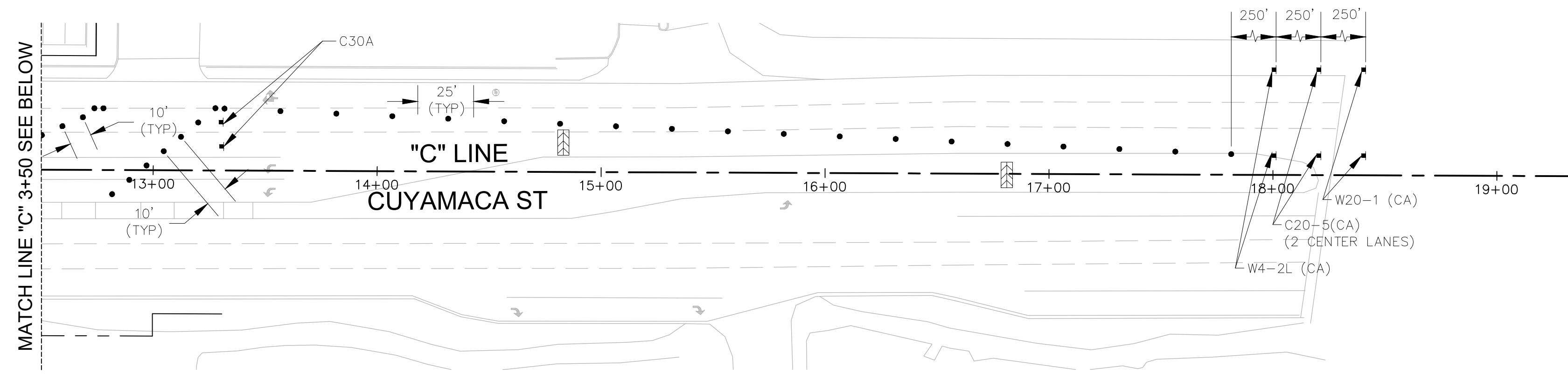
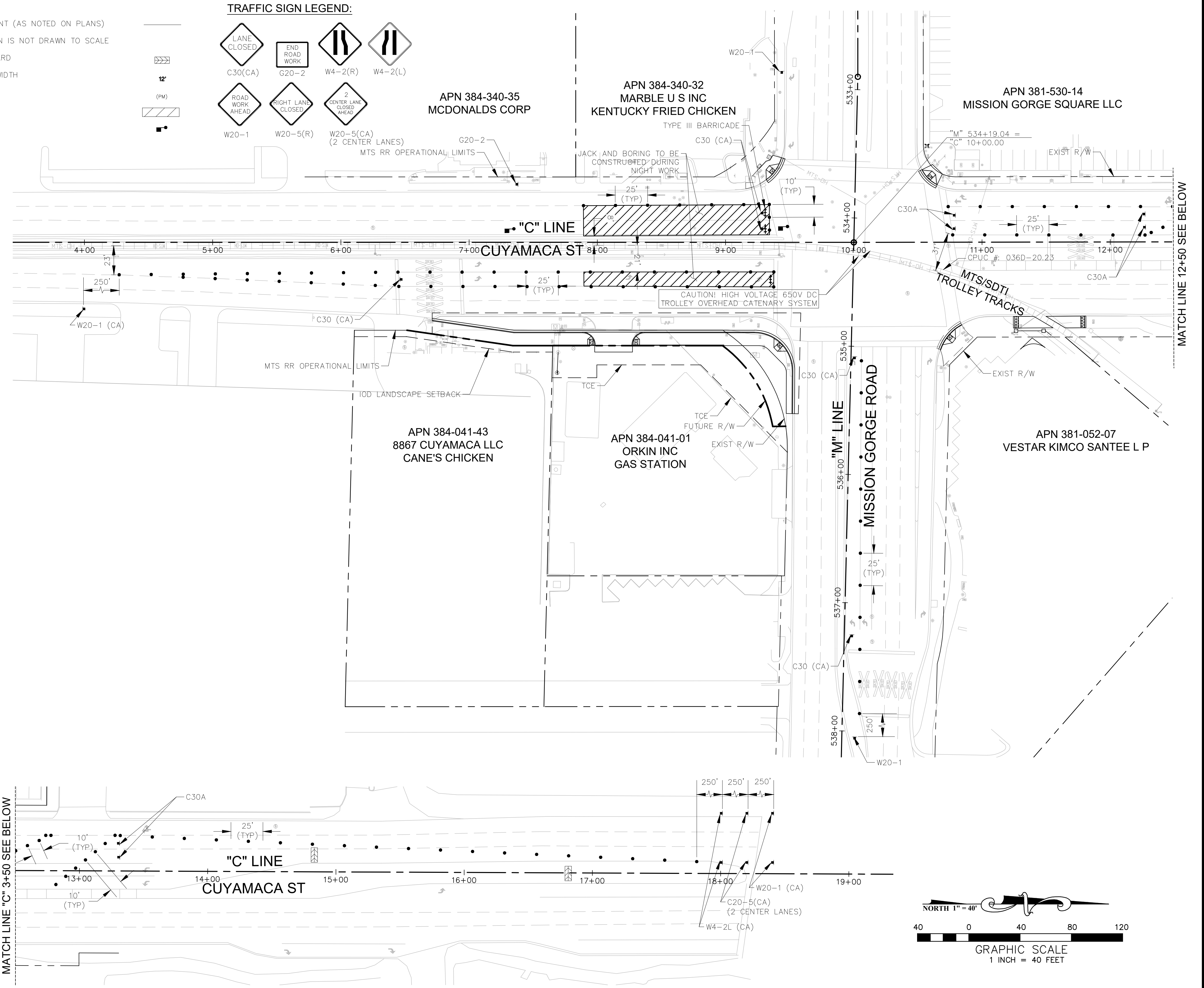
- LANE STRIPING - PAINT (AS NOTED ON PLANS)
- NTS - LENGTH SHOWN IS NOT DRAWN TO SCALE
- FLASHING ARROW BOARD
- CONSTRUCTION LINE WIDTH
- POST MOUNTED SIGN
- WORK AREA
- MTS FLAGPERSON

**TRAFFIC SIGN LEGEND:**

- LANE CLOSED
- END ROAD WORK
- W4-2(R)
- W4-2(L)
- ROAD WORK AHEAD
- EIGHT LANE CLOSED
- 2 CENTER LANE CLOSED AHEAD

**CONSTRUCTION NOTES**

1. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE CALIFORNIA MUTCD, (MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES) LATEST EDITION.
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5. TRENCHES MUST BE BACK FILLED OR PLATED DURING NON-WORKING HOURS.
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7. TEMPORARY "NO PARKING" SIGNS WILL BE POSTED 48 HOURS PRIOR TO COMMENCING WORK FOR THAT PHASE.
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14. ALL CONSTRUCTION SIGNS SHALL BE INSTALLED ON TYPE II BARRICADES OR ON 4" X 4" WOOD POSTS IN THE GROUND.
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18. TEMPORARY PAINTED STRIPING AND SIGNAGE MUST BE USED WHEN TRAFFIC PATTERNS ARE CHANGED. ALL CONFLICTING STRIPING AND SIGNS SHALL BE REMOVED PRIOR TO THE CHANGE IN THE TRAFFIC PATTERNS.
19. SIGNS SHOWN ARE MINIMUM REQUIRED. ADDITIONAL SIGNS SHALL BE INSTALLED AS DIRECTED BY THE CITY TRAFFIC ENGINEER OR AS REQUIRED.
20. CONTRACTOR SHALL SUBMIT TRAFFIC CONTROL PLANS TO THE CITY 10 WORKING DAYS IN ADVANCE FOR REVIEW AND APPROVAL FOR ITEMS OF WORK NOT COVERED BY THESE TRAFFIC CONTROL PLANS.
21. THE PHASING SHOWN ON THESE PLANS MAY BE MODIFIED BY THE CONTRACTOR. IF THE PHASING IS CHANGED, THE CONTRACTOR SHALL SUBMIT THEIR OWN PHASING/TRAFFIC PLANS TO THE CITY 10 WORKING DAYS IN ADVANCE FOR REVIEW AND APPROVAL PRIOR TO THE START OF CONSTRUCTION.
22. ALL WORK REQUIRING LANE CLOSURES NIGHT WORK 9:00 PM TO 5:00 AM.
23. MTS RIGHT OF ENTRY PERMIT REQUIRED FOR ALL WORK, TRAFFIC CONTROL, EQUIPMENT LOCATED WITHIN 15' OF OUTER TRACK.
24. FOR ANY WORK ADJACENT TO RAILS, CONTRACTOR TO REFER TO THE REQUIREMENTS NOTED IN THE CALIFORNIA MUTCD, PART 8, TRAFFIC CONTROL FOR RAILROAD AND LIGHT RAIL TRANSIT GRADE CROSSINGS



**DOKKEN ENGINEERING**  
1400 PRADDER ROAD  
SUITE 100  
SAN JOSE, CA 95128  
(408) 514-8777

CONTRACTOR	INSPECTOR	DATE COMPLETED	CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK	SCALE	DESIGNED BY	DRAWN BY	CHECKED BY	REVIEWED	CITY OF SANTEE	DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO.	DRAWING NO.
									STATION 2015 PER RGS 11302 - LEAD PLUG & BRASS TAG STAMPED "RCE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST RETURN AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.	HORIZ: 1" = 40' VERT: N/A	VINCENT MALONG	VINCENT MALONG	MARK TARRALL		CITY OF SANTEE	DEPARTMENT OF DEVELOPMENT SERVICES	CIP 2014-02	2021-111
											PLANS PREPARED UNDER THE SUPERVISION OF MARK TARRALL DATE 01/12/2024 RCE NO. 71953 EXPIRES 12/31/2025		TRAFFIC HANDLING PLAN - JACK AND BORE CUYAMACA STREET RIGHT TURN POCKET		SHEET 12 OF 21			

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024

**NOTES:**

1. ALL EXISTING STRIPING IN CONFLICT WITH PROPOSED STRIPING SHALL BE SANDBLASTED.
2. ALL NEW TRAFFIC SIGNAL DEVICES SHOWN ON THIS PLAN ARE FOR REFERENCE ONLY. SEE TRAFFIC SIGNAL PLAN SHEETS FOR CONSTRUCTION WORK TO BE DONE.
3. ALL NEW SURFACE IMPROVEMENTS SHOWN ON THIS PLAN ARE FOR REFERENCE ONLY. SEE PLAN AND PROFILE SHEETS FOR CONSTRUCTION WORK TO BE DONE.
4. ALL NEW STREET LIGHTS AND ASSOCIATED FACILITIES SHOWN ON THIS PLAN ARE FOR REFERENCE ONLY. SEE LIGHTING PLAN FOR CONSTRUCTION WORK TO BE DONE.
5. LIMIT LINES AND CROSSWALKS SHALL BE FIELD LOCATED AND APPROVED BY THE CITY TRAFFIC ENGINEER PRIOR TO INSTALLATION..
6. OBTAIN STREET NAME SIGN TEMPLATE/LAYOUT FROM CITY PRIOR TO MANUFACTURING STREET NAME SIGNS.
7. REMOVE AND REPLACE DISTURBED STRIPING 300' ALL DIRECTIONS.

**STRIPING LEGEND**

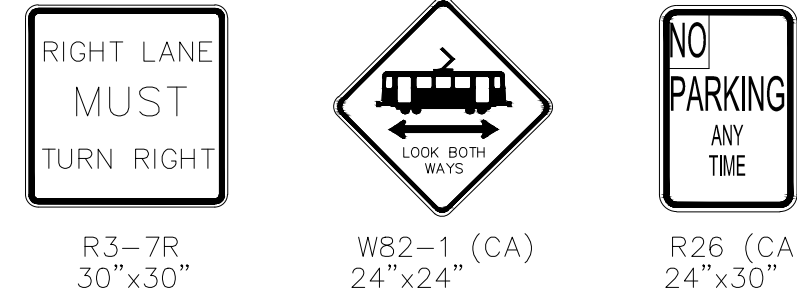
- ◇ TYPE IV (R) ARROW PER CSP A24B
- ◇ 12" WHITE LINE LIMIT LINE (STOP LINE) PER CSP A24G
- ◇ TYPE IV (L) ARROW PER CSP A24A
- ◇ RAILROAD CROSSING SYMBOL
- ◇ 12" WHITE LINE PER CSP A24F CROSSWALK
- ◇ "LOOK BOTH WAYS" PAVEMENT MARKING PER DETAIL ON SHEET 5.
- ◇ CSP A20 STRIPING DETAIL NUMBER

**REMOVAL STRIPING LEGEND**

- R1 REMOVE TYPE III (R) ARROW PER CSP A24B
- R2 REMOVE 12" WHITE LINE (STOP LINE) PER CSP A24G
- R3 REMOVE TYPE IV ARROW PER CSP A24A
- R4 REMOVE RAILROAD CROSSING SYMBOL
- R5 REMOVE 12" WHITE LINE CROSSWALK PER CSP A24F
- REM REMOVE CSP A20 STRIPING DETAIL NUMBER

**SIGN LEGEND**

- PROPOSED ROADWAY SIGN OR STREET NAME SIGN



APN 384-340-32  
MARBLE U S INC  
KENTUCKY FRIED CHICKEN

APN 381-530-14  
MISSION GORGE SQUARE LLC

APN 384-340-35  
MCDONALDS CORP

APN 384-041-43  
8867 CUYAMACA LLC  
CANE'S CHICKEN

APN 384-041-01  
ORKIN INC  
GAS STATION

APN 381-052-07  
VESTAR KIMCO SANTEE L P

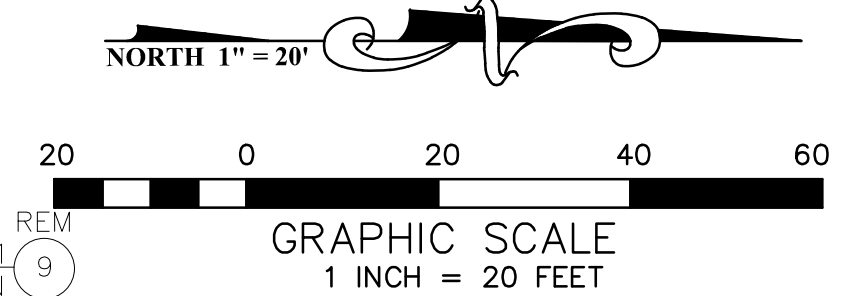
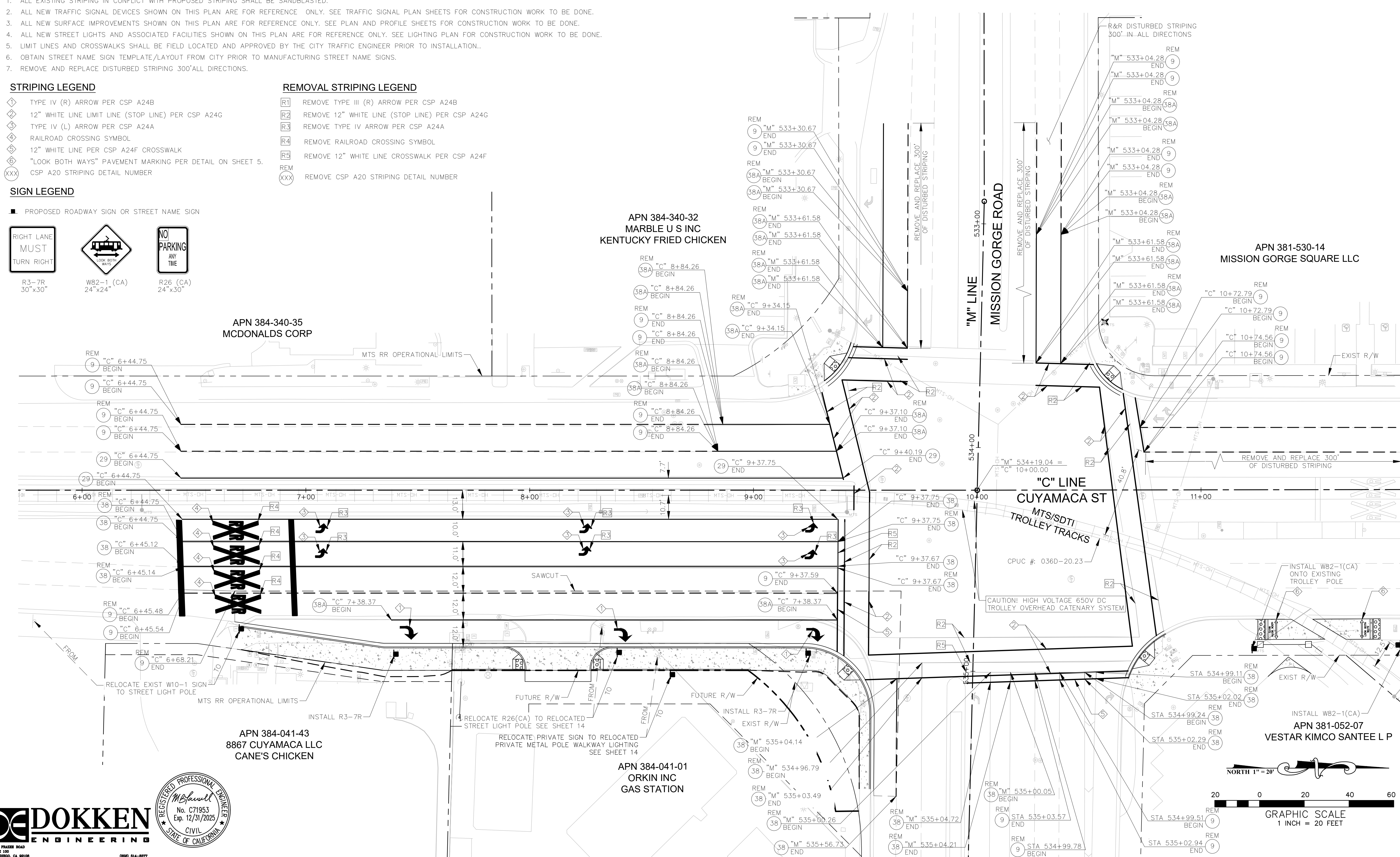


CONTRACTOR	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK
INSPECTOR						STATION 2015 PER RGS 11392 - LEAD PILE & BRASS TAG STAMPED "RCE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.
DATE COMPLETED						ELEVATION: 337.97, M.S.L. DATUM

SCALE	DESIGNED BY	DRAWN BY	CHECKED BY	REVIEWED
HORIZ: 1" = 20'	VINCENT MALONG	VINCENT MALONG	MARK TARRALL	
VERT: N/A	PLANS PREPARED UNDER THE SUPERVISION OF MARK TARRALL DATE 01/12/2024 RCE NO. 71953 EXPIRES 12/31/2025			

CITY OF SANTEE	DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO.	DRAWING NO.
PAVEMENT DELINEATION AND SIGN PLAN CUYAMACA STREET RIGHT TURN POCKET		CIP 2014-02	2021-112
PROJECT ENGINEER			SHEET 13 OF 21

CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD



100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024

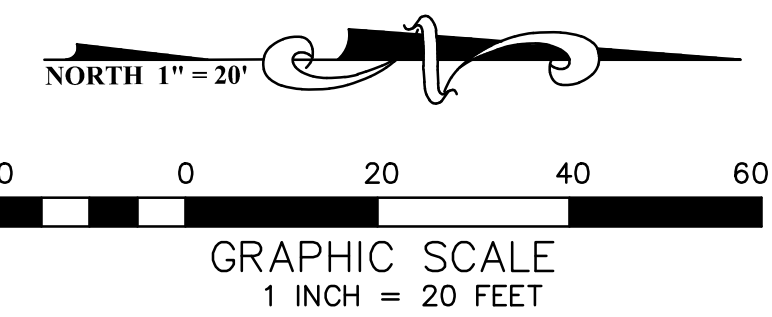
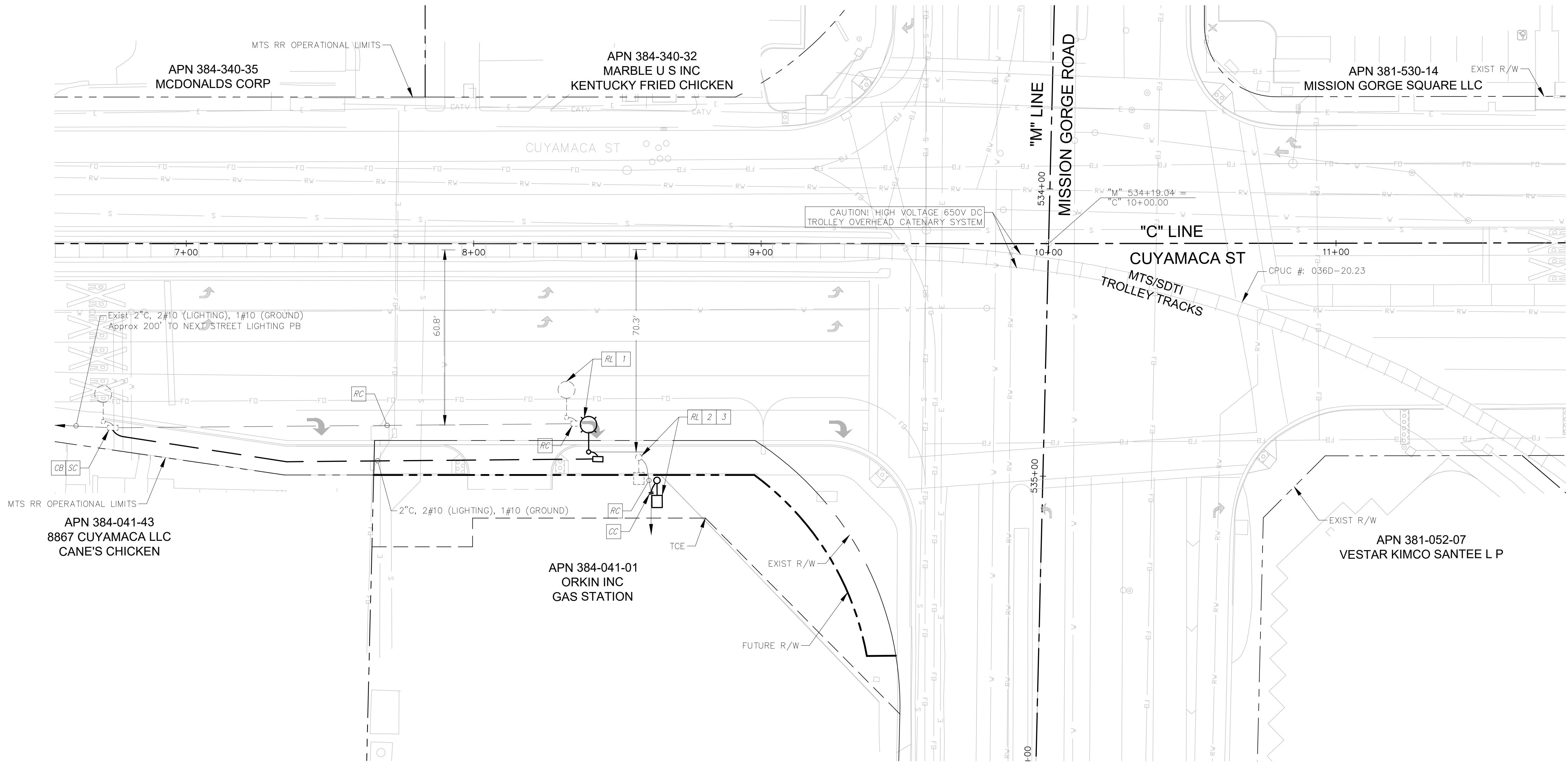


**LEGEND**

- CB INSTALL CONDUIT INTO EXISTING PULL BOX
- CC CONNECT NEW AND EXISTING CONDUIT, REMOVE EXISTING CONDUCTORS AND INSTALL CONDUCTORS AS INDICATED
- RC EQUIPMENT OR MATERIAL TO BE REMOVED SHALL BE REMOVED AND DISPOSED BY THE CONTRACTOR
- RL RELOCATE EQUIPMENT.
- SC SPLICE NEW TO EXISTING CONDUCTORS
- PB PULL BOX

**CONSTRUCTION NOTES**

- 1 RELOCATED STREET LIGHT PER SANTEE PW-20.
- 2 RELOCATED WALKWAY LIGHT PER SANTEE PW-17.
- 3 SPLICE EXISTING WALKWAY LIGHTING CONDUCTORS INTO NEW WALKWAY LIGHT LOCATION.



**DOKKEN ENGINEERING**  
 1400 PRAIRIE ROAD  
 SUITE 100  
 SAN DIEGO, CA 92108  
 (619) 514-8777

CONTRACTOR	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK STATION 2075 PER RDS 11202 - LEAD PLUG & BRASS TAG STAMPED "RCE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.  ELEVATION: 337.97, M.S.L. DATUM	SCALE HORIZ: 1" = 20' VERT: N/A	DESIGNED BY JOSE OSTDIEK	DRAWN BY JENNIFER KOBRYA	CHECKED BY JOSE OSTDIEK	REVIEWED _____  BY _____ PROJECT ENGINEER	CITY OF SANTEE LIGHTING PLAN CUYAMACA STREET RIGHT TURN POCKET	DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO. CIP 2014-02	DRAWING NO. 2021-113 SHEET 14 OF 21
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100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024

Loc	POLE AND EQUIPMENT SCHEDULE									
	STANDARD TYPE	MAST Sig	ARMS Ltg	Lum INDUCTION	SIGNAL MAST ARM	POLE	PED	Ø	APPS ARROW	SPECIAL REQUIREMENTS
(A)	61-5-100	60'	15'	150W	3-MAS	SV-2-TB	SP-2-T	2P	→	RELOCATE EXIST STANDARD END EXIST EQUIPMENT TO NEW LOCATION
(B)	OCS POLE	-	-	-	-	LRT	-	-	-	EXIST STANDARD AND EQUIPMENT TO REMAIN
(C)	29-5-80	50'	15'	150W	2-MAS	SV-1-T	SP-1-T	2P	→	EXIST STANDARD AND EQUIPMENT TO REMAIN
(D)	1-A	-	-	-	-	TV-1-T	SP-1-T	-	-	EXIST STANDARD AND EQUIPMENT TO REMAIN
(E)	29-5-80	50'	15'	150W	3-MAS	SV-1-T	SP-1-T	-	-	EXIST STANDARD AND EQUIPMENT TO REMAIN
(F)	1-A	-	-	-	-	TV-1-T	SP-1-T	4P	→	EXIST STANDARD AND EQUIPMENT TO REMAIN
(G)	TYPE 33	5'	-	-	MAS	LRT	-	-	-	EXIST STANDARD AND EQUIPMENT TO REMAIN
(H)	OCS POLE	-	-	-	-	LRT	-	-	-	EXIST STANDARD AND EQUIPMENT TO REMAIN
(I)	19-3-80	25'	15'	150W	MAS	SV-2-TB	SP-1-T	6P	→	EXIST STANDARD AND EQUIPMENT TO REMAIN
(J)	1-A	-	-	-	-	TV-2-T	SP-1-T	8P	→	EXIST STANDARD AND EQUIPMENT TO REMAIN

**LEGEND**

- AB ABANDON, IF APPLIED TO CONDUIT, REMOVE CONDUCTORS
- BC REPLACE PULL BOX IN EXISTING CONDUIT RUN WITH A #6 PULL BOX OR AS SHOWN
- CB INSTALL CONDUIT INTO EXISTING PULL BOX.
- CC CONNECT NEW AND EXISTING CONDUIT. REMOVE EXISTING CONDUCTORS AND INSTALL CONDUCTORS AS INDICATED
- RC EQUIPMENT OR MATERIAL TO BE REMOVED SHALL BE REMOVED AND DISPOSED BY THE CONTRACTOR
- RS REMOVE AND SALVAGE EQUIPMENT
- RL RELOCATE EQUIPMENT

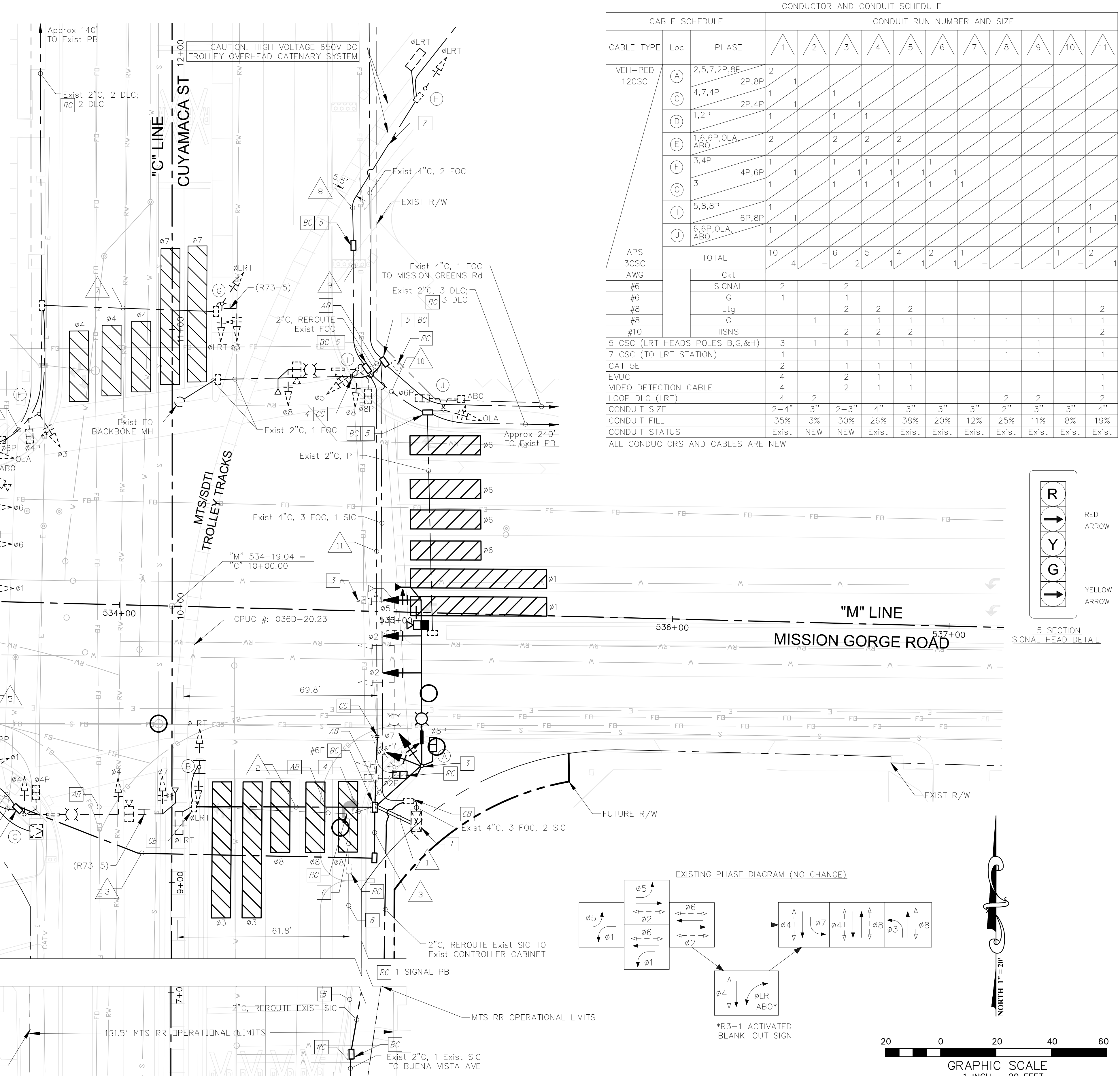
- FOC FIBER OPTIC CABLE
- PB PULL BOX
- ABO BLANK-OUT SIGN

**NOTES**

1. ALL EXISTING DETECTOR LOOPS EXCEPT TROLLEY DETECTION ARE NOT USED.
2. ALL EXISTING EQUIPMENT IS TO REMAIN UNLESS OTHERWISE SHOWN.
3. CONTRACTOR SHALL RE-GROUT THE BOTTOM OF ALL PULL BOXES.
4. REMOVE ALL UNUSED DLC.
5. ABANDONED CONDUITS SHALL BE REMOVED FROM PULL BOXES.
6. FULL REMIRE OF THE INTERSECTION AND THAT ALL WORK MUST BE COMPLETED AT NIGHT IN A SINGLE 8-HOUR WINDOW.
7. OLD POLE FOUNDATION SHALL BE DEMOLISHED 18" BELOW GRADE.
8. EXISTING RR INTERCONNECTION TO MTS SIGNAL SYSTEM TO BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. IF DAMAGED AT ANYTIME, CONTRACTOR MUST CONTACT MTS EMERGENCY RESPONSE IMMEDIATELY: (619) 595-4960.

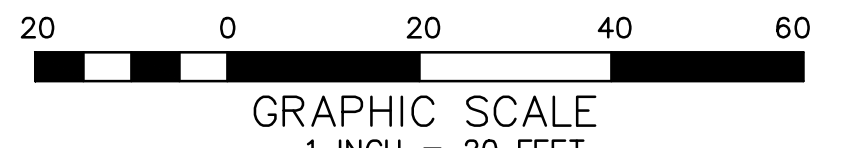
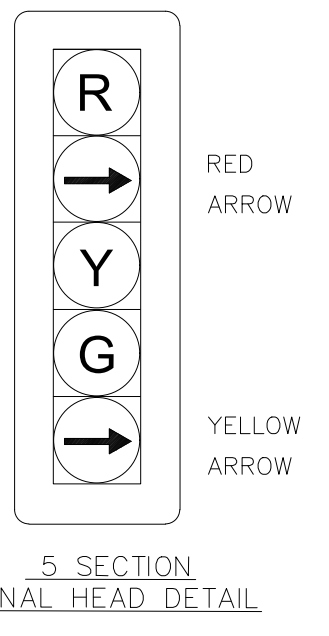
**CONSTRUCTION NOTES**

- 1 EXISTING 333JP CABINET WITH 170E CONTROLLER ASSEMBLY TO REMAIN.
- 2 EXISTING TYPE III-B SERVICE EQUIPMENT ENCLOSURE WITH THE FOLLOWING BREAKERS TO REMAIN:  
 METERED:  
 120V 50A 1P - SIGNAL  
 120V 15A 1P - IISNS  
 UNMETERED:  
 240V 30A 2P - INTERSECTION LIGHTING
- 3 RELOCATE EXISTING SIGNAL STANDARD AND EXISTING EQUIPMENT TO NEW FOUNDATION PER 2002 CALTRANS STANDARD PLANS. VERIFY EXISTING BOLT CIRCLE SIZE AND PATTERN PRIOR TO CONSTRUCTING FOUNDATION.
- 4 REROUTE EXISTING FOC TO NEW CONDUIT, ABANDON EXISTING CONDUIT.
- 5 RC EXISTING PULL BOX.
- 6 REROUTE EXISTING SIC TO NEW CONDUIT, ABANDON EXISTING CONDUIT.
- 7 CSC FOR LRT TO REMAIN AND PROTECT IN PLACE.



CABLE SCHEDULE		CONDUIT AND CONDUIT SCHEDULE												
CABLE TYPE	Loc	PHASE	1	2	3	4	5	6	7	8	9	10	11	
VEH-PED 12CSC	(A)	2,5,7,2P,8P	2											
	(C)	4,7,4P	1	1										
	(D)	1,2P	1		1	1								
	(E)	1,6,6P,OLA, ABO	2		2	2	2							
	(F)	3,4P	1	1	1	1	1	1	1	1				
	(G)	3	1		1	1	1	1	1	1				
	(I)	5,8,8P	1										1	
	(J)	6,6P,OLA, ABO	1										1	
	TOTAL			10	4	6	5	4	2	1	-	-	1	2
	APPS 3CSC													
AWG	Ckt													
#6	SIGNAL		2		2									
#6	G		1		1									
#8	Ltg				2	2							2	
#8	G			1	2	1	1	1	1	1	1	1	1	
#10	IISNS				2	2	2						2	
5 CSC (LRT HEADS POLES B,G,&H)			3	1	1	1	1	1	1	1	1	1	1	
7 CSC (TO LRT STATION)			1								1	1	1	
CAT 5E			2		1	1	1							
EVUC			4		2	1	1						1	
VIDEO DETECTION CABLE			4		2	1	1						1	
LOOP DLC (LRT)			4	2							2	2	2	
CONDUIT SIZE			2-4"	3"	2-3"	4"	3"	3"	3"	2"	3"	3"	4"	
CONDUIT FILL			35%	3%	30%	26%	38%	20%	12%	25%	11%	8%	19%	
CONDUIT STATUS			Exist	NEW	NEW	Exist	Exist	Exist	Exist	Exist	Exist	Exist	Exist	

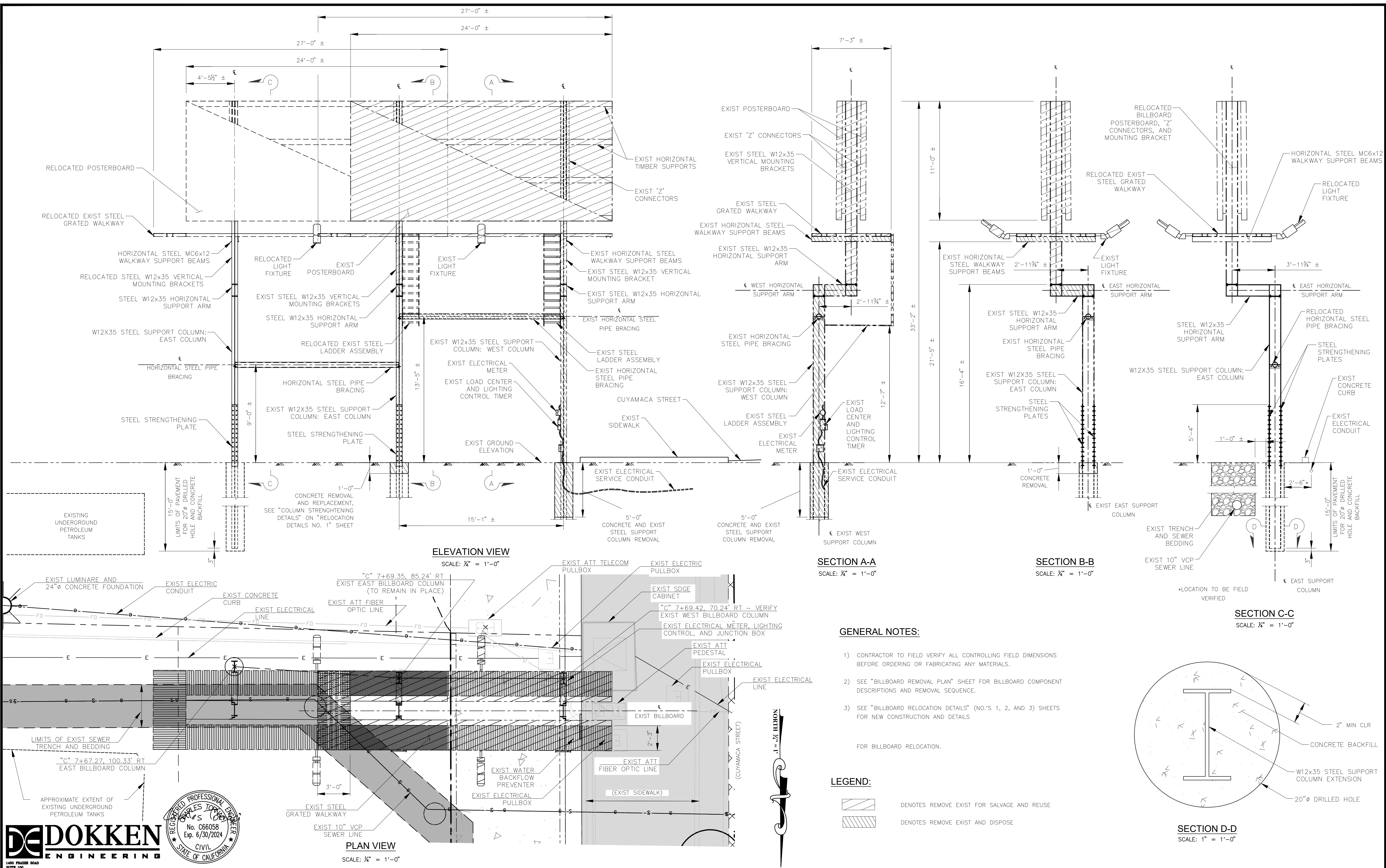
ALL CONDUCTORS AND CABLES ARE NEW



CONTRACTOR	INSPECTOR	DATE COMPLETED	CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK	STATION 2015 PER ROS 11202 - LEAD PILE & BRASS TAG STAMPED "ICE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.	SCALE	DESIGNED BY	DRAWN BY	CHECKED BY	REVIEWED	CITY OF SANTEE	DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO.	DRAWING NO.
									ELEVATION: 337.97, M.S.L. DATUM		HORIZ: 1" = 20'	JOSE OSTDIEK	JENNIFER KOBRYA	JOSE OSTDIEK		TRAFFIC SIGNAL PLAN		CIP 2014-02	2021-114
											VERT: N/A	PLANS PREPARED UNDER THE SUPERVISION OF	JOSE OSTDIEK	DATE 01/12/2024	BY PROJECT ENGINEER	CUYAMACA STREET RIGHT TURN POCKET			15 OF 21
												RCE NO. 65334	DATE EXPIRES 9/30/2025						

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 03/04/2024





- GENERAL NOTES:**
- 1) CONTRACTOR TO FIELD VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIALS.
  - 2) SEE "BILLBOARD REMOVAL PLAN" SHEET FOR BILLBOARD COMPONENT DESCRIPTIONS AND REMOVAL SEQUENCE.
  - 3) SEE "BILLBOARD RELOCATION DETAILS" (NO.'S 1, 2, AND 3) SHEETS FOR NEW CONSTRUCTION AND DETAILS.
- FOR BILLBOARD RELOCATION.

**LEGEND:**

	DENOTES REMOVE EXIST FOR SALVAGE AND REUSE
	DENOTES REMOVE EXIST AND DISPOSE

**DOKKEN ENGINEERING**  
 1400 PRAIRIE ROAD  
 SUITE 100  
 SAN DIEGO, CA 92108  
 (619) 514-8777

REGISTERED PROFESSIONAL ENGINEER  
 CHARLES TORNA CI  
 No. C66058  
 Exp. 6/30/2024  
 CIVIL  
 STATE OF CALIFORNIA

CONTRACTOR	CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK STATION 2075 PER RDS 1132 - LEAD PLUG & BRASS TAG STAMPED "RCE 1693" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.	SCALE HORIZ: 1" = 5' VERT: N/A	DESIGNED BY CHARLES TORNA CI	DRAWN BY ERIC PONCE	CHECKED BY CHARLES TORNA CI	REVIEWED	CITY OF SANTEE DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO. CIP 2014-02	DRAWING NO. 2021-115
INSPECTOR							ELEVATION: 337.97, M.S.L. DATUM		PLANS PREPARED UNDER THE SUPERVISION OF CHARLES TORNA CI	DATE 07/26/22	EXPIRES 6/30/2024	BY PROJECT ENGINEER	GENERAL PLAN CUYAMACA STREET RIGHT TURN POCKET		SHEET 16 OF 21

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 07/26/2022



**STRUCTURE NOTES:**

**SUBMITTALS**

- PRIOR TO REMOVING ANY PORTION OF THE EXISTING BILLBOARD OR SUPPORT STRUCTURE, THE CONTRACTOR SHALL SUBMIT A DEMOLITION PLAN IN CONFORMANCE WITH SECTION 2-5.3, "SUBMITTALS," OF THE STANDARD SPECIFICATIONS. THE DEMOLITION PLAN SHALL INCLUDE DETAILS FOR THE FOLLOWING:
  - REMOVAL SEQUENCE, INCLUDING STAGING OF REMOVAL ACTIVITIES AND EQUIPMENT LOCATIONS
  - TEMPORARY SUPPORT SHORING OR BRACING
  - LOCATIONS WHERE WORK IS PERFORMED NEAR TRAFFIC OR UTILITIES
  - PROTECTION OF PEOPLE, PROPERTY, UTILITIES, AND IMPROVEMENTS
  - METHODS FOR PREVENTING MATERIAL, EQUIPMENT, AND DEBRIS FROM FALLING ONTO TRAFFIC.
- PRIOR TO PROCUREMENT OR FABRICATION OF ANY STRUCTURAL STEEL, THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS IN CONFORMANCE WITH SECTION 304-1.1.1, "SHOP DRAWINGS," OF THE STANDARD SPECIFICATIONS.
- PRIOR TO ERECTION OF STRUCTURAL STEEL OR RECONSTRUCTION OF ANY PORTION OF THE BILLBOARD OR SUPPORT STRUCTURE, THE CONTRACTOR SHALL SUBMIT AN ERECTION PLAN IN CONFORMANCE WITH SECTIONS 2-5.3, "SUBMITTALS," AND 304-1.1.2, "FALSEWORK PLANS," OF THE STANDARD SPECIFICATIONS. THE ERECTION PLAN SHALL INCLUDE DETAILS FOR THE FOLLOWING:
  - METHODS AND EQUIPMENT IN ACCORDANCE WITH SECTION 304-1.2, "METHODS AND EQUIPMENT," OF THE STANDARD SPECIFICATIONS
  - ERECTION SEQUENCE, INCLUDING STAGING OF MATERIALS AND EQUIPMENT LOCATIONS
  - TEMPORARY SUPPORT SHORING OR BRACING, IF REQUIRED
  - LOCATIONS WHERE WORK IS PERFORMED NEAR TRAFFIC OR UTILITIES
  - PROTECTION OF PEOPLE, PROPERTY, UTILITIES, AND IMPROVEMENTS
  - METHODS FOR PREVENTING MATERIAL, EQUIPMENT, AND DEBRIS FROM FALLING ONTO TRAFFIC.

**CONSTRUCTION NOTES:**

- ALL MATERIALS SHOWN ON THE PLANS TO BE REMOVED AND DISPOSED OF SHALL BE DISPOSED OFF SITE IN ACCORDANCE WITH SECTION 300-1.3, "REMOVAL AND DISPOSAL OF MATERIALS," OF THE STANDARD SPECIFICATIONS.
- PRIOR TO DRILLING HOLES FOR BILLBOARD FOUNDATIONS, LOCATE ALL UTILITIES WITHIN 10 FEET OF THE PROPOSED HOLE.
- DRILLED HOLES FOR BILLBOARD COLUMN FOUNDATIONS SHALL CONFORM WITH SECTIONS 205-3, "CONCRETE PILES," 205-3.3.2, "PILES CAST IN DRILLED HOLES," AND 305-1.3.2, "DRILLED HOLES FOR CAST-IN-PLACE PILES," OF THE STANDARD SPECIFICATIONS.
- REINFORCING STEEL FOR DRILL AND GROUT DOWELS SHALL BE GRADE 60 AND SHALL CONFORM WITH SECTION 201-2, "REINFORCEMENT FOR CONCRETE," OF THE STANDARD SPECIFICATIONS.
- CONCRETE FOR BILLBOARD FOUNDATIONS SHALL BE CONSIDERED SPECIFIED BY CLASS, AS DEFINED IN SECTION 201-1.1.2, "CONCRETE SPECIFIED BY CLASS AND ALTERNATE CLASS," OF THE STANDARD SPECIFICATIONS, AND SHALL CONFORM AS SPECIFIED FOR "CAST-IN-PLACE PILES" IN TABLE 201-1.1.2 OF THE STANDARD SPECIFICATIONS.
- AT CONSTRUCTION JOINT BETWEEN EXISTING AND NEW CONCRETE IN EXISTING EAST COLUMN FOUNDATION, ROUGHEN SURFACE TO AN AMPLITUDE OF 1/4-INCH. PREPARE CONSTRUCTION JOINT IN ACCORDANCE WITH SECTION 303-1.8.6, "JOINTS," OF THE STANDARD SPECIFICATIONS.
- ALL NEW STEEL FOR RECONSTRUCTION OF THE BILLBOARD SHALL BE CONSIDERED "STRUCTURAL STEEL" IN ACCORDANCE WITH SECTIONS 206-1, "STRUCTURAL STEEL, RIVETS, BOLTS, PINS AND ANCHOR BOLTS," AND 304-1, "STRUCTURAL STEEL," OF THE STANDARD SPECIFICATIONS AND SHALL CONFORM TO ALL REQUIREMENTS THEREIN.
- STRUCTURAL STEEL PLATES AND ROLLED SHAPES SHALL CONFORM TO ASTM A709 GR 50. ROUND HOLLOW STRUCTURAL SECTIONS SHALL CONFORM TO ASTM A500, GRADE B OR C.
- ALL NEW STRUCTURAL STEEL FOR RECONSTRUCTION OF THE BILLBOARD SHALL BE HOT-DIP GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH SECTION 210-3, "GALVANIZING," OF THE STANDARD SPECIFICATIONS. REPAIR OF DAMAGED AREAS OF GALVANIZING, AND GALVANIZING OF EXISTING STEEL SURFACES WHERE SHOWN ON THE PLANS, SHALL CONFORM TO SECTION 210-3.5, "REPAIR OF DAMAGED ZINC COATINGS," OF THE STANDARD SPECIFICATIONS.
- DRILL AND GROUT DOWELS SHALL CONFORM TO SECTIONS 51-1.02G, "GROUT," AND 51-1.03E(4), "DRILL AND GROUT DOWELS," OF THE STATE OF CALIFORNIA STANDARD SPECIFICATIONS, 2018. CURE GROUT USING CURING COMPOUND OR WATER CURING METHOD IN ACCORDANCE WITH SECTION 303-1.10, "CURING," OF THE STANDARD SPECIFICATIONS.
- NEW ELECTRICAL CONDUIT SHALL BE CONFORM WITH SECTION 700-3.5.2, "RIGID METALLIC CONDUIT," OF THE STANDARD SPECIFICATIONS.

**GENERAL NOTES:**

- CONTRACTOR TO FIELD VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIALS.
- SEE "BILLBOARD REMOVAL PLAN" SHEET FOR BILLBOARD COMPONENT DESCRIPTIONS AND REMOVAL SEQUENCE.
- SEE "BILLBOARD RELOCATION DETAILS" (NO.'S 1, 2 AND 3) SHEETS FOR NEW CONSTRUCTION AND DETAILS FOR BILLBOARD RELOCATION.

**GENERAL NOTES  
LOAD AND RESISTANCE FACTOR DESIGN**

**DESIGN:**  
AASHTO LFRD LTS (SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS), 1ST EDITION DATED 2015 WITH 2017, 2018, 2019, AND 2020 INTERIM REVISIONS

**DEAD LOAD:**  
SELF-WIRGHT OF STRUCTURAL STEEL AND ATTACHED APPURTENANCES

**WIND LOAD:**  
AASHTO LFRD LTS (SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS), 1ST EDITION

$V_{EXTREME} = 110$  MPH  
 $V_{SERVICE} = 72$  MPH

**LIVE LOAD:**  
WALKWAY LIVE LOADING OF 500LBS POINT LOAD

**CONCRETE BACKFILL:**  
F<sub>C</sub> = 3.6 KSI  
F<sub>Y</sub> = 60 KSI  
N = 8

**STRUCTURAL STEEL:**  
STM A709 GRADE 50  
F<sub>Y</sub> = 50 KSI  
F<sub>U</sub> = 65 KSI

**WELDS:**  
AWS D1.5

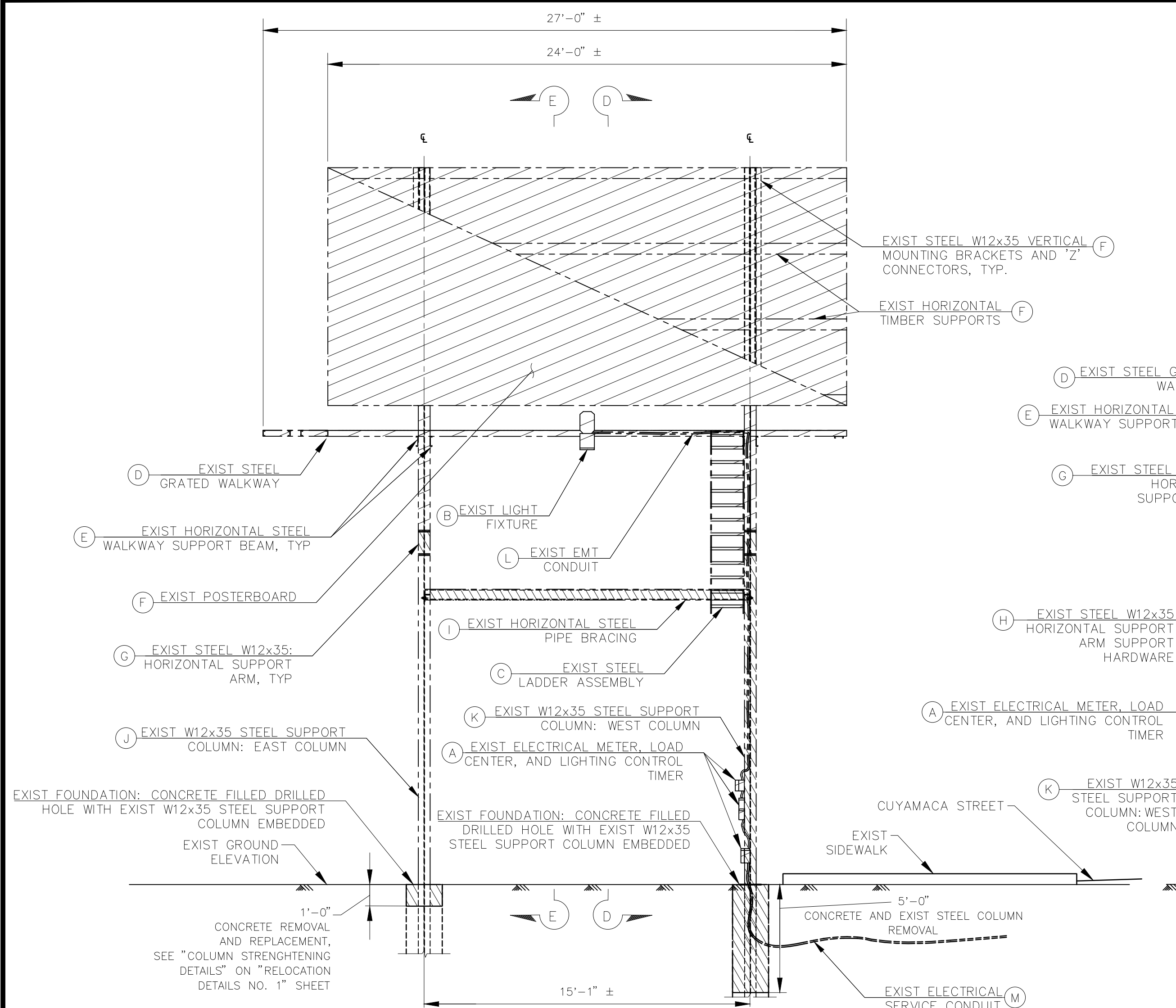
**HIGH STRENGTH BOLTS:**  
ASTM A325 HIGH STRENGTH BOLTS:  
F<sub>Y</sub> = 58 KSI  
F<sub>U</sub> = 81 KSI



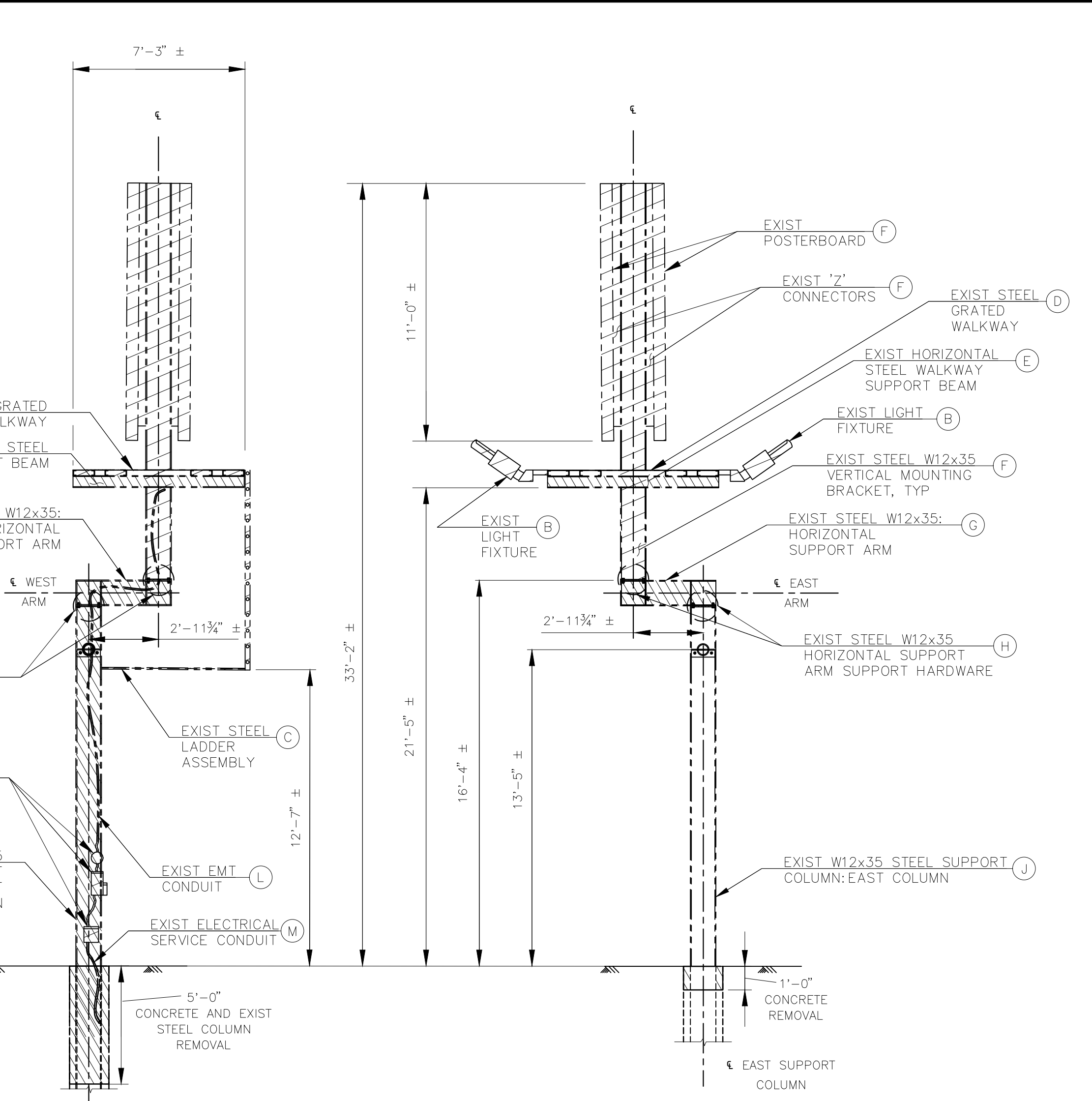
1400 PRAIRIE ROAD  
SUITE 100  
SAN DIEGO, CA 92108  
(619) 514-8777

CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK	SCALE	DESIGNED BY	DRAWN BY	CHECKED BY	REVIEWED	CITY OF SANTEE	DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO.	DRAWING NO.	
						STATION 2015 PER R08 11302 - LEAD PILE & BRASS TAG STAMPED "RICE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST RETURN AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.	HORIZ: 1" = 5'	CHARLES TORNACI	ERIC PONCE	CHARLES TORNACI						
						ELEVATION: 337.97, M.S.L. DATUM	VERT: N/A	PLANS PREPARED UNDER THE SUPERVISION OF CHARLES TORNACI DATE 07/26/22			BY _____	STRUCTURE SPECIFICATIONS		CIP 2014-02	2021-116	
								RCE NO. 66058		EXPIRES 6/30/2024	PROJECT ENGINEER	CUYAMACA STREET RIGHT TURN POCKET			SHEET 17 OF 21	

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 07/26/2022

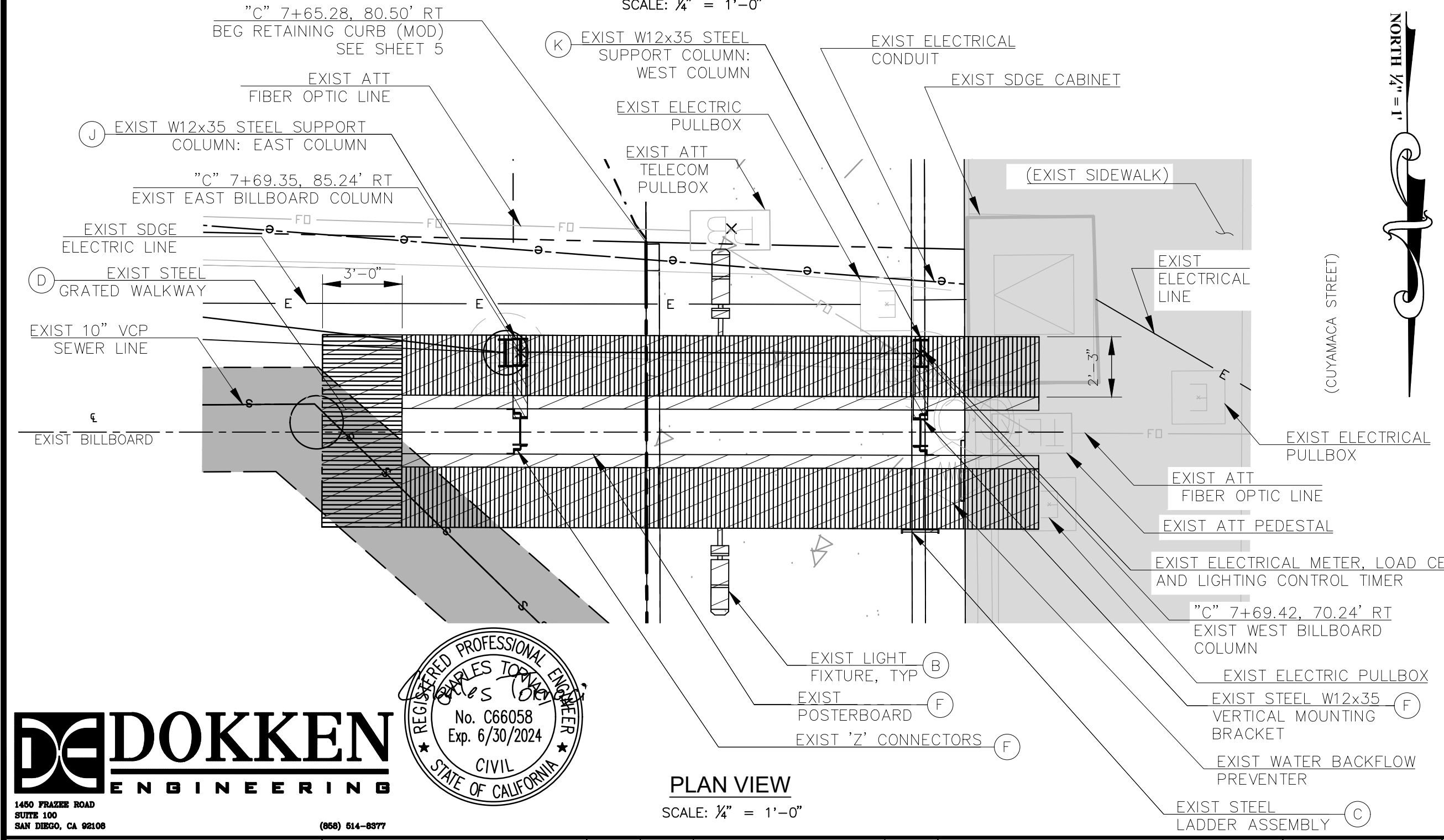


**ELEVATION VIEW**  
SCALE: 1/4" = 1'-0"



**SECTION D-D**  
SCALE: 1/4" = 1'-0"

**SECTION E-E**  
SCALE: 1/4" = 1'-0"



**PLAN VIEW**  
SCALE: 1/4" = 1'-0"

**REMOVAL SEQUENCE:**

- CONTACT SDG&E FOR ELECTRICAL SERVICE DISCONNECT TO DE-ENERGIZE EXISTING ELECTRICAL METER.
- DISCONNECT ELECTRICAL METER, LOAD CENTER, LIGHTING CONTROL TIMER, AND EMT CONDUIT TO EXISTING BILLBOARD LIGHTING.
- REMOVE AND SALVAGE EXISTING POWER SUPPLY BOX (TO BE RE-ATTACHED TO EXISTING EAST COLUMN TO REMAIN).
- REMOVE AND SALVAGE EXISTING LIGHT FIXTURES AND ATTACHMENT HARDWARE (TO BE RE-ATTACHED AND RELOCATED TO STEEL GRATED WALKWAY).
- REMOVE AND SALVAGE EXISTING STEEL LADDER ASSEMBLY (TO BE RE-ATTACHED AND RELOCATED TO GRATED STEEL WALKWAY).
- REMOVE AND SALVAGE EXISTING STEEL GRATED WALKWAY (TO BE RE-ATTACHED AND RELOCATED TO NEW STEEL W12x35 HORIZONTAL SUPPORT ARMS AND VERTICAL MOUNTING BRACKETS).
- REMOVE EXISTING HORIZONTAL STEEL WALKWAY SUPPORT BEAMS.
- REMOVE AND SALVAGE EXISTING BILLBOARD SYSTEM AS A SINGLE UNIT (TO BE RE-ATTACHED TO RECONFIGURED BILLBOARD SUPPORT ARMS).
- REMOVE EXISTING STEEL W12x35 HORIZONTAL SUPPORT ARMS AND SUPPORT HARDWARE.
- REMOVE EXISTING HORIZONTAL STEEL BRACING.
- REMOVE EXISTING WEST STEEL SUPPORT COLUMN.
- REMOVE EAST CONCRETE FOUNDATION TO EAST STEEL W12x35 SUPPORT COLUMN TO 1'-0" BELOW EXISTING GRADE. PROTECT EXISTING EAST STEEL W12x35 SUPPORT COLUMN IN PLACE.
- REMOVE WEST CONCRETE FOUNDATION TO WEST STEEL W12x35 SUPPORT COLUMN TO 3'-0" BELOW EXISTING GRADE.

**REMOVAL COMPONENT NOTES:**

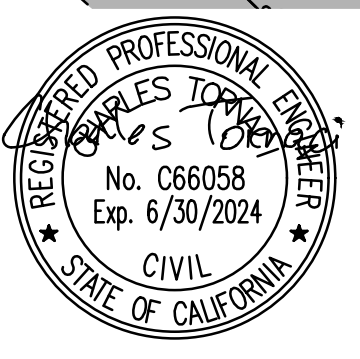
- A. EXISTING ELECTRICAL METER, LOAD CENTER, AND LIGHTING CONTROL TIMER TO BE REMOVED AND REPLACED FOR RE-ATTACHMENT TO EXISTING W12x35 STEEL COLUMN PER SDG&E WORK ORDER. TO REMAIN IN PLACE.
- B. EXISTING LIGHT FIXTURES AND ATTACHMENT HARDWARE. TO BE REMOVED AND SALVAGED FOR RE-ATTACHMENT TO RELOCATED GRATED STEEL WALKWAY.
- C. EXISTING STEEL LADDER ASSEMBLY. TO BE REMOVED AND SALVAGED FOR RE-ATTACHMENT TO RELOCATED GRATED STEEL WALKWAY.
- D. EXISTING STEEL GRATED WALKWAY. TO BE REMOVED AND SALVAGED FOR RE-ATTACHMENT TO RECONFIGURED BILLBOARD SYSTEM.
- E. EXISTING HORIZONTAL STEEL WALKWAY SUPPORT BEAMS. TO BE REMOVED.
- F. EXISTING BILLBOARD SYSTEM (POSTERBOARD, HORIZONTAL TIMBER SUPPORTS, STEEL W12x35 VERTICAL MOUNTING BRACKETS, AND 'Z' CONNECTORS). TO BE REMOVED AS A SINGLE UNIT AND SALVAGED TO RE-ATTACHMENT TO NEW SUPPORT COLUMNS.
- G. EXISTING STEEL W12x35 HORIZONTAL SUPPORT ARM. TO BE REMOVED.
- H. EXISTING STEEL W12x35 HORIZONTAL SUPPORT ARM SUPPORT HARDWARE. TO BE REMOVED.
- I. EXISTING HORIZONTAL STEEL PIPE BRACING. TO BE REMOVED.
- J. EXISTING STEEL W12x35 SUPPORT COLUMN: EAST COLUMN. PROTECT IN PLACE.
- K. EXISTING STEEL W12x35 SUPPORT COLUMN: WEST COLUMN. TO BE REMOVED.
- L. EXISTING EMT CONDUIT. TO BE REMOVED. NEW EMT CONDUIT TO BE INSTALLED TO EXISTING W12x35 STEEL SUPPORT COLUMN TO REMAIN IN PLACE.
- M. EXISTING ELECTRICAL SERVICE CONDUIT. REMOVE AND REPLACE PER SDG&E SERVICE ORDER.

**GENERAL NOTES:**

- ALL CONTROLLING FIELD DIMENSIONS TO BE VERIFIED BY CONTRACTOR BEFORE ORDERING/FABRICATING ANY MATERIAL.
- CONTRACTOR MUST SUBMIT WORKING DRAWINGS OF BILLBOARD REMOVAL PLAN. DEVIATIONS FROM REMOVAL SEQUENCE MUST BE APPROVED BY THE ENGINEER.
- CONTRACTOR MUST SUBMIT WORKING DRAWINGS DETAILING BILLBOARD ERECTION SEQUENCE PLAN.

**LEGEND:**

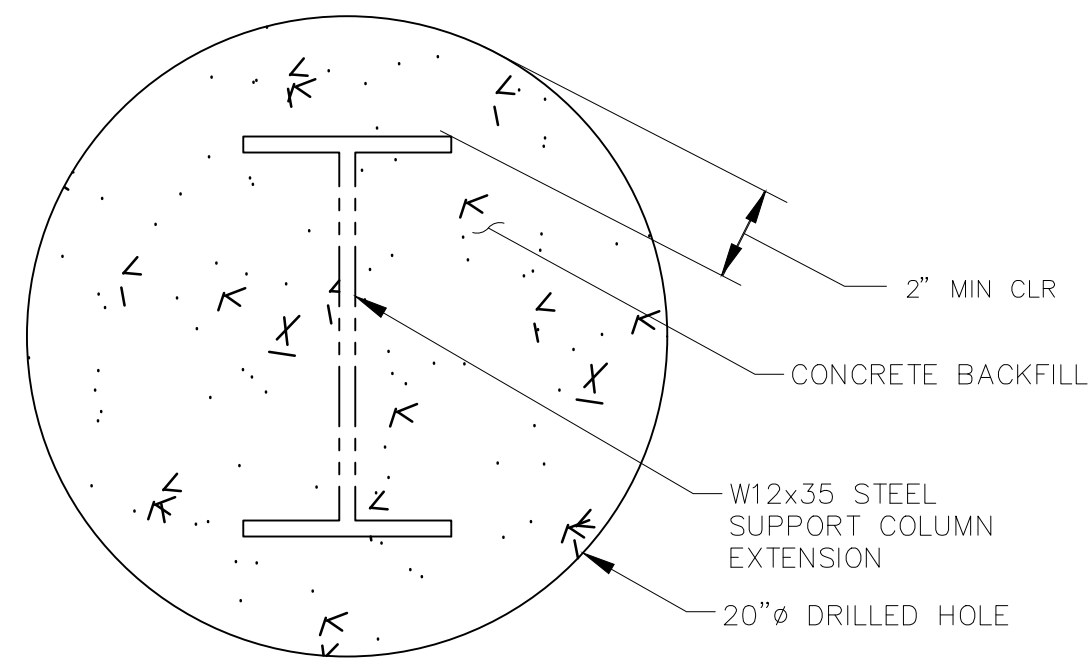
- DENOTES REMOVE EXIST FOR SALVAGE AND REUSE
- DENOTES REMOVE EXIST AND DISPOSE



CONTRACTOR	CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK	SCALE	DESIGNED BY	DRAWN BY	CHECKED BY	REVIEWED	CITY OF SANTEE	DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO.	DRAWING NO.
INSPECTOR							STATION 2015 PER R08 11262 - LEAD PLUG & BRASS TAG STAMPED "RCE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.	HORIZ: 1" = 5'	CHARLES TORNACI	ERIC PONCE	CHARLES TORNACI		BILLBOARD REMOVAL PLAN		CIP 2014-02	2021-117
DATE COMPLETED							ELEVATION: 337.97, M.S.L. DATUM	VERT: N/A	PLANS PREPARED UNDER THE SUPERVISION OF	CHARLES TORNACI	DATE 07/26/22	BY	CUYAMACA STREET RIGHT TURN POCKET			SHEET 18 OF 21
									RCE NO. 66058	DATE EXPIRES 6/30/2024	PROJECT ENGINEER					

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 07/26/2022

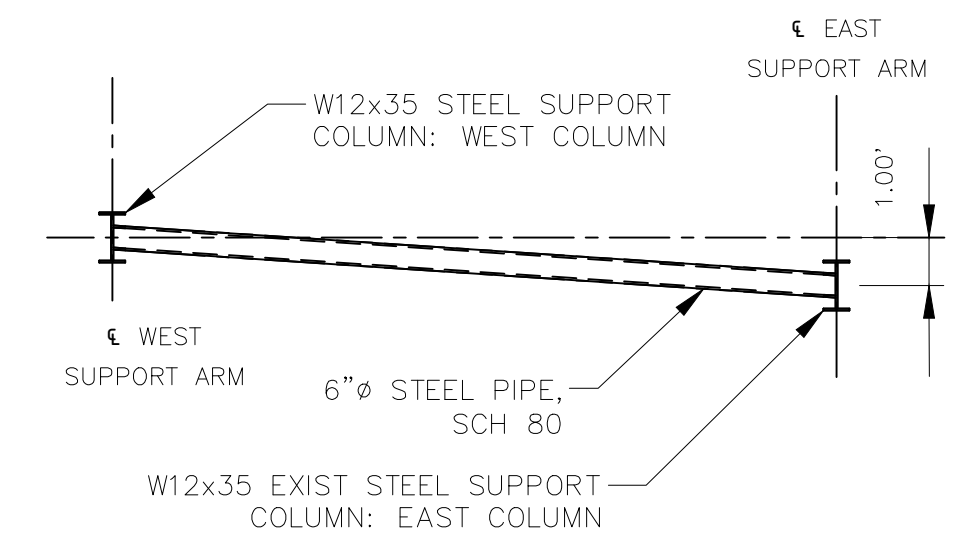




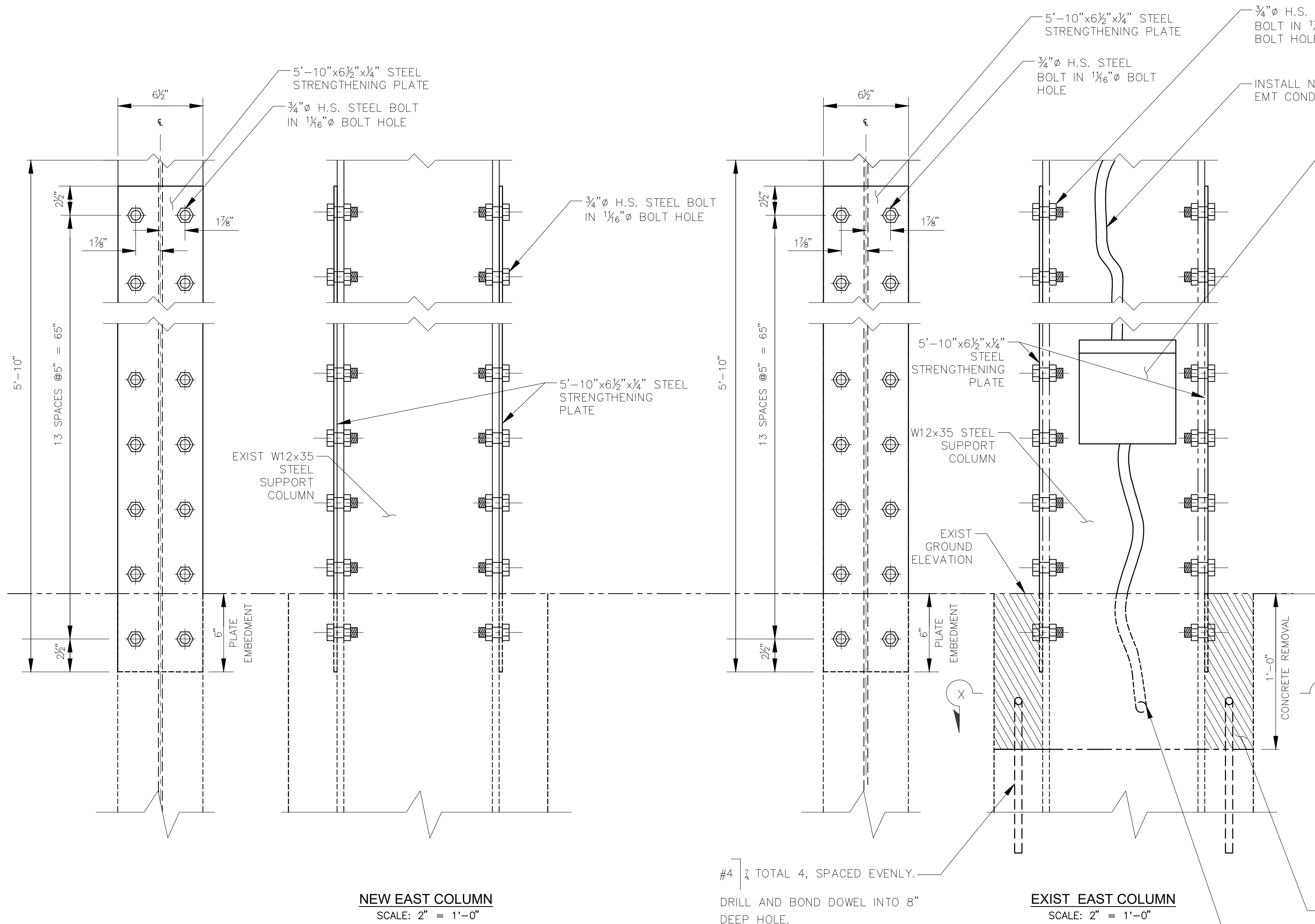
**SECTION X-X**  
SCALE: 2" = 1'-0"

**NOTES:**

- ON SITE FIELD DRILLING OF STRUCTURAL STEEL WILL BE REQUIRED.
- FIELD APPLY COLD GALVANIZING AT EXIST EAST COLUMN TO REMAIN FROM 1'-0" ABOVE TO 1'-0" BELOW EXIST GROUND ELEVATION
- ALL NEW STRUCTURAL STEEL COLUMNS, PLATE, BOLTS, NUTS, AND HARDWARE SHALL BE HOT DIPPED GALVANIZED. STRENGTHENING PLATES SHALL BE HOT DIPPED GALVANIZED AFTER HOLES HAVE BEEN DRILLED.
- TIGHTEN STRENGTHENING PLATE BOLTS TO 28 KIPS TENSION FOR A SLIP CRITICAL CONNECTION AT THE SUPPORT COLUMN - STRENGTHENING PLATE INTERFACE.



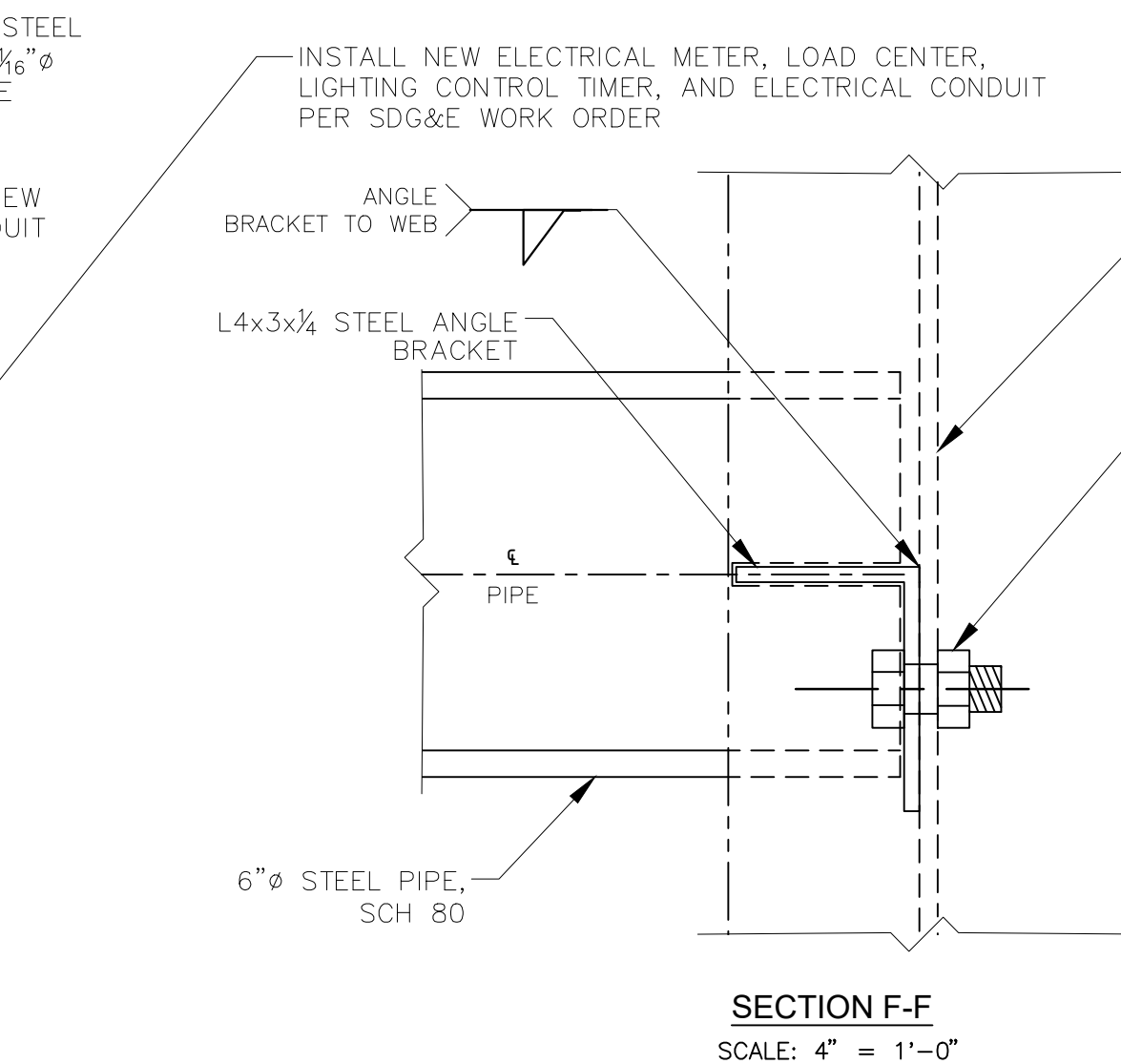
**PLAN VIEW**  
SCALE: 1/4" = 1'-0"



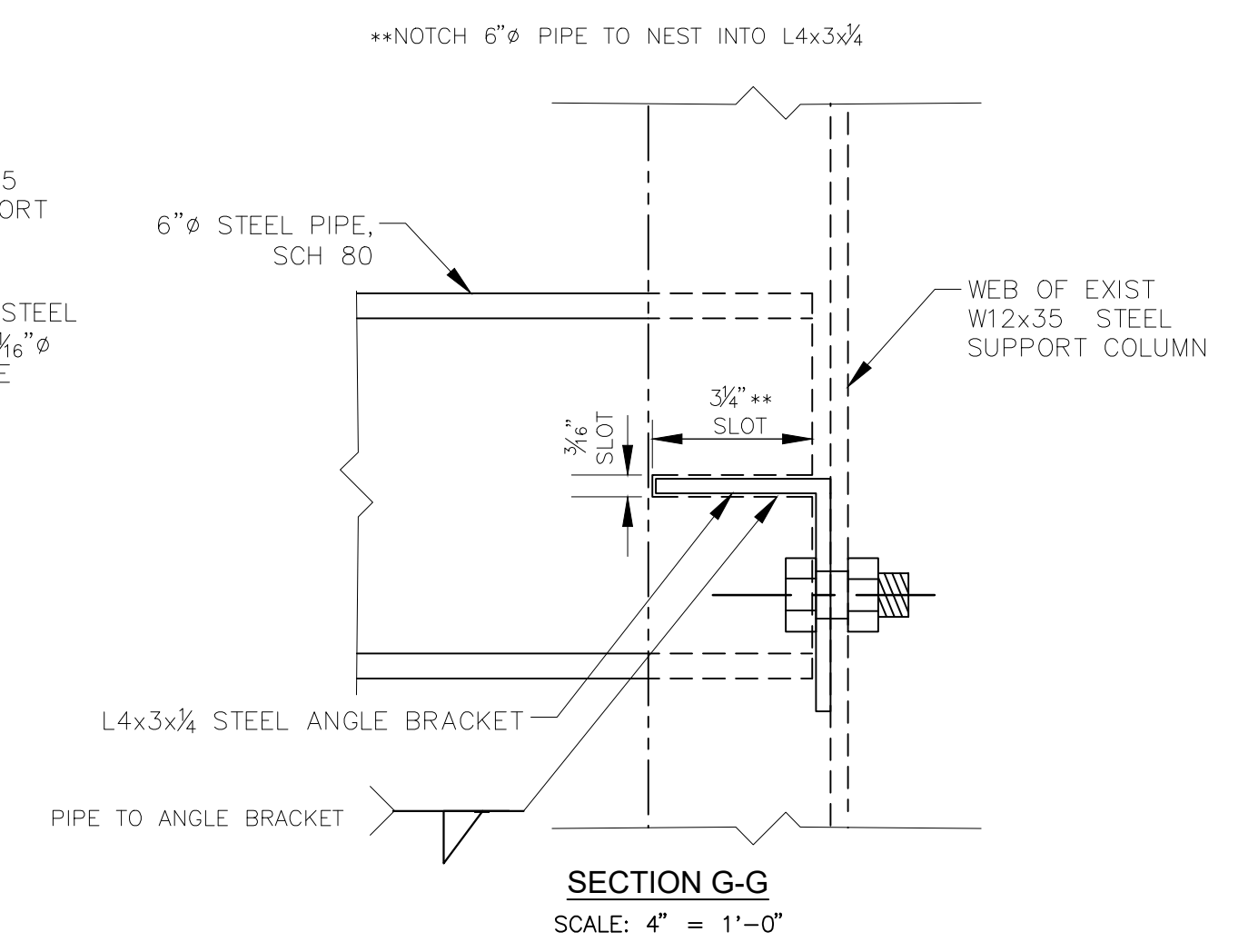
**NEW EAST COLUMN**  
SCALE: 2" = 1'-0"

**EXIST EAST COLUMN**  
SCALE: 2" = 1'-0"

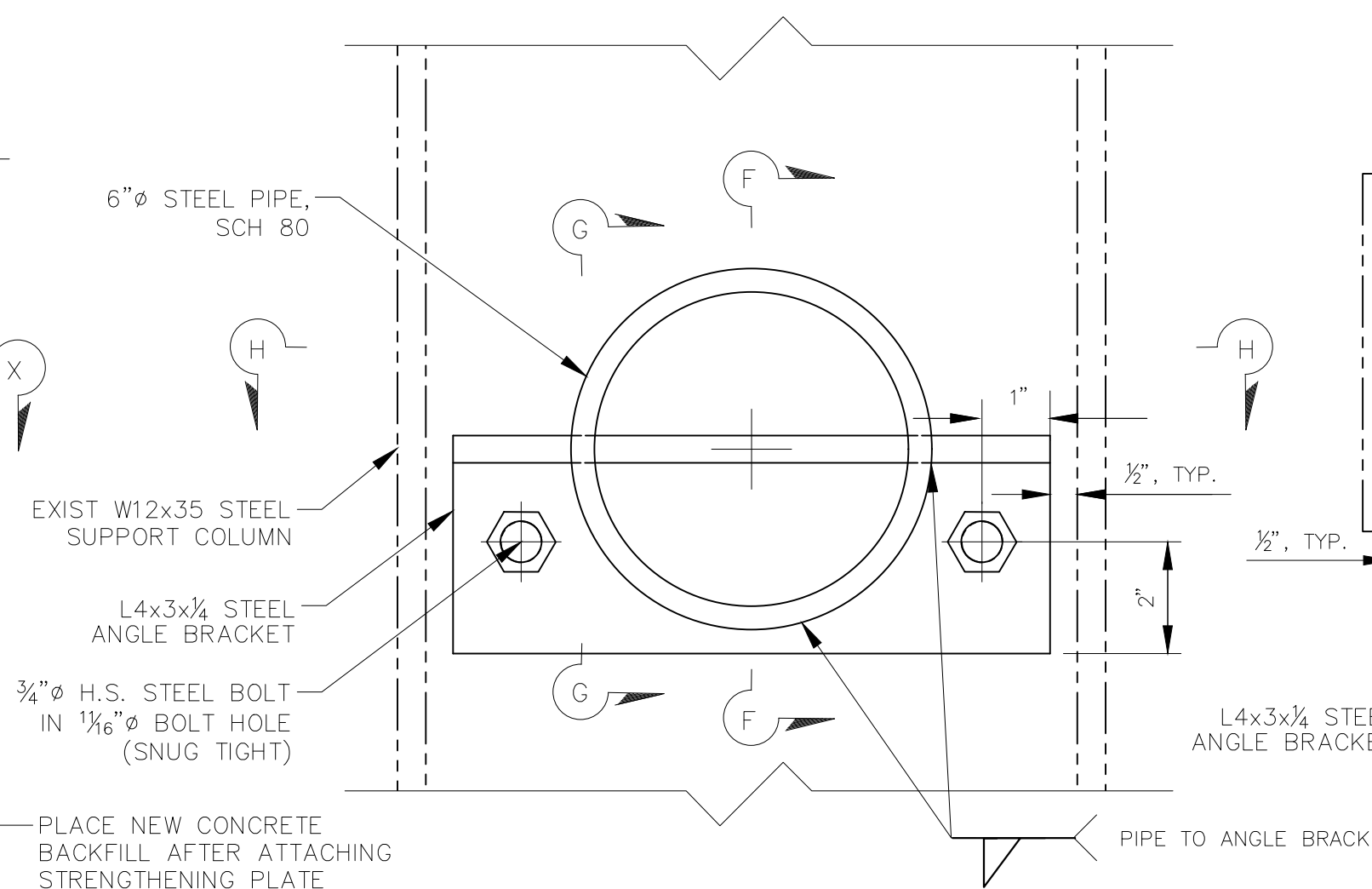
**COLUMN STRENGTHENING DETAILS**



**SECTION F-F**  
SCALE: 4" = 1'-0"



**SECTION G-G**  
SCALE: 4" = 1'-0"



**SECTION H-H**  
SCALE: 4" = 1'-0"

**HORIZONTAL PIPE AND BRACKET CONNECTION**  
SCALE: 4" = 1'-0"

**HORIZONTAL STEEL PIPE BRACING DETAILS**

**LEGEND:**

DENOTES REMOVE EXIST AND DISPOSE



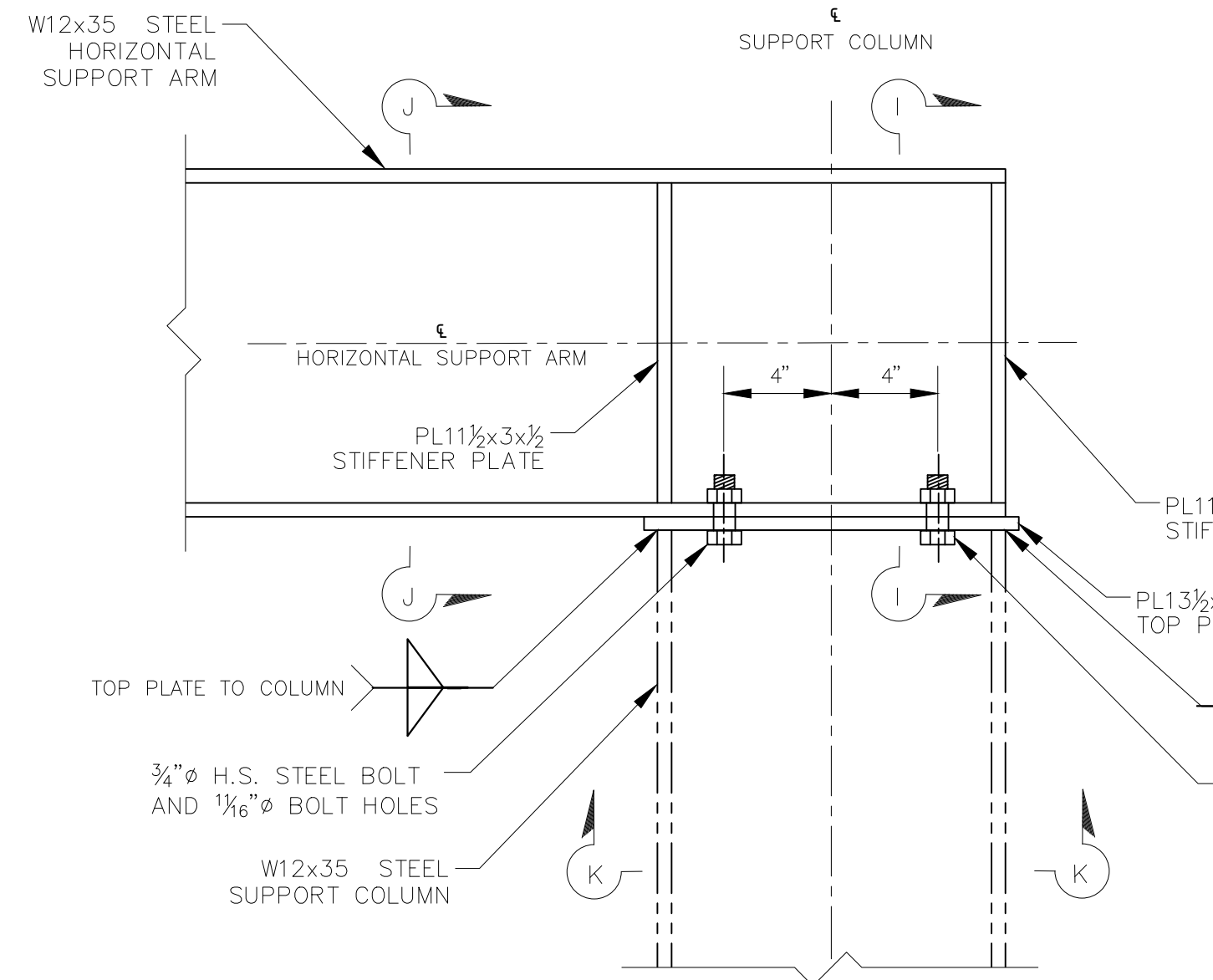
CONTRACTOR	INSPECTOR	DATE COMPLETED	CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD	BENCH MARK
									STATION 2075 PER R08 11202 - LEAD PLUG & BRASS TAG STAMPED "RCE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.

SCALE	DESIGNED BY	DRAWN BY	CHECKED BY
HORIZ: 1" = 5'	CHARLES TORNACI	ERIC PONCE	CHARLES TORNACI
VERT: N/A	PLANS PREPARED UNDER THE SUPERVISION OF CHARLES TORNACI		
	RCE NO. 66058	DATE 07/26/22	EXPIRES 6/30/2024

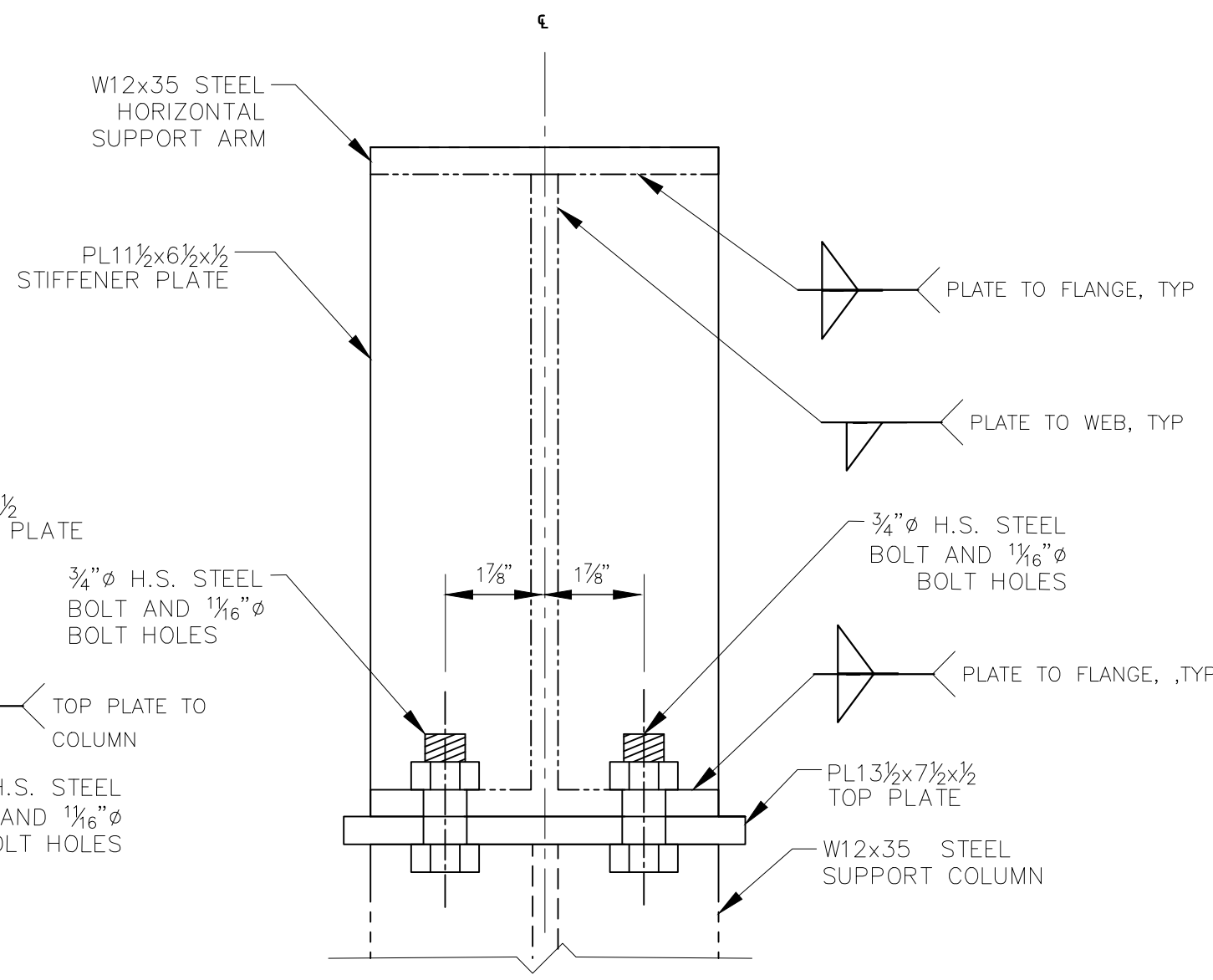
REVIEWED	BY
	PROJECT ENGINEER

CITY OF SANTEE	DEPARTMENT OF DEVELOPMENT SERVICES	CITY W.O. NO.	DRAWING NO.
RELOCATION DETAILS NO. 1 CUYAMACA STREET RIGHT TURN POCKET		CIP 2014-02	2021-118

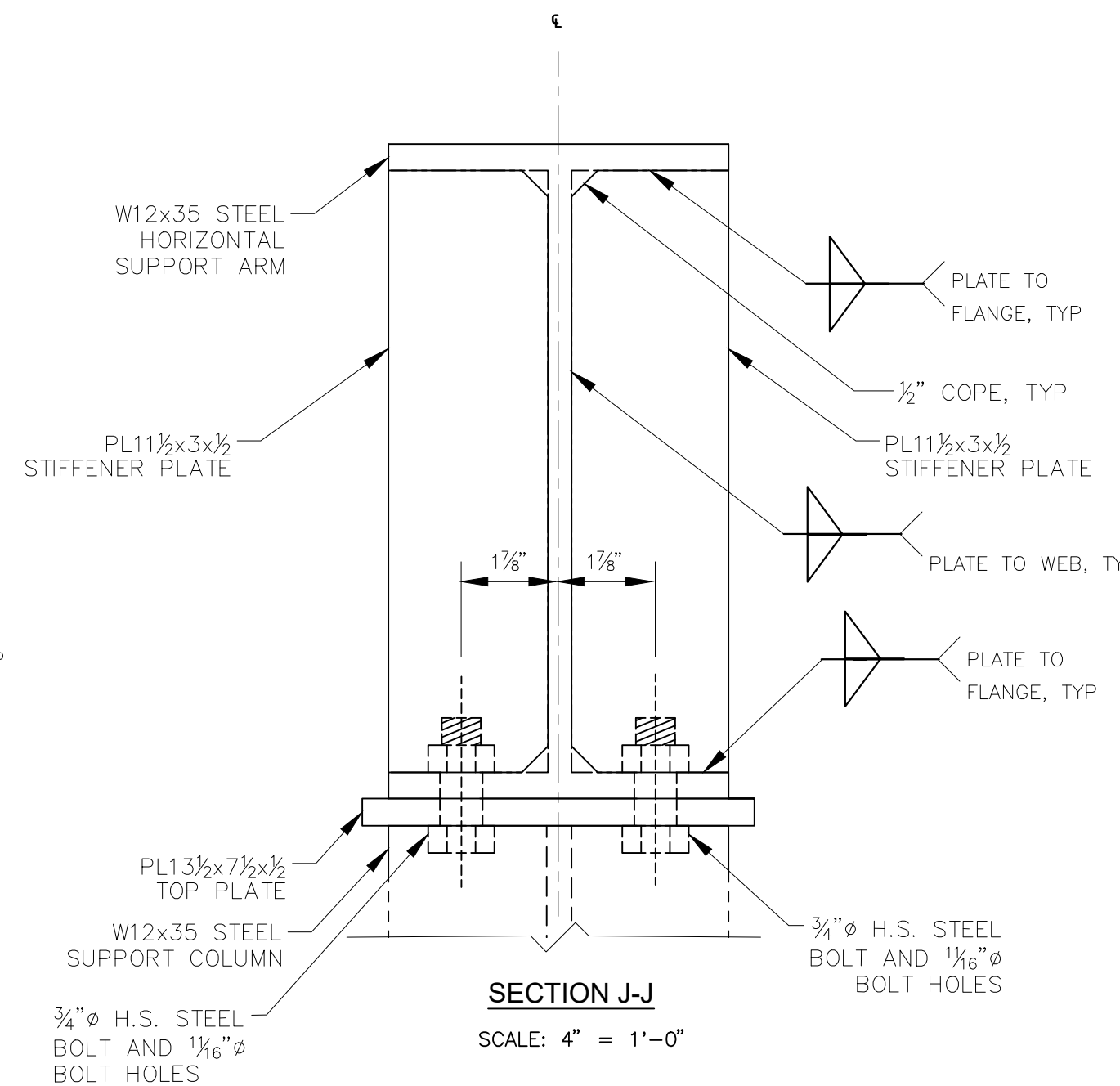
CIP 2014-02	2021-118
SHEET 19 OF 21	



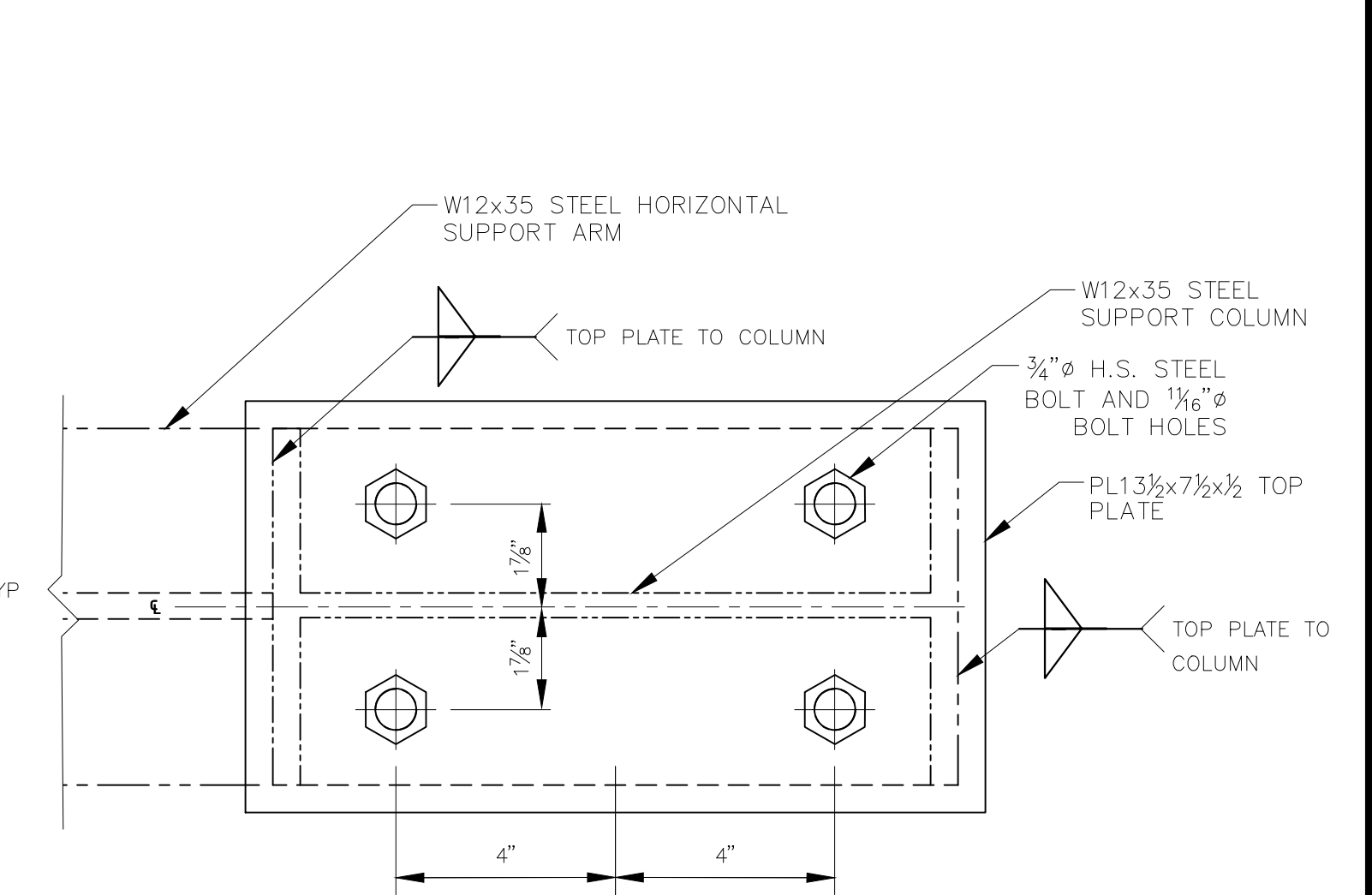
**SUPPORT COLUMN TO HORIZONTAL SUPPORT ARM CONNECTION DETAIL**  
SCALE: 2" = 1'-0"



**SECTION I-I**  
SCALE: 4" = 1'-0"

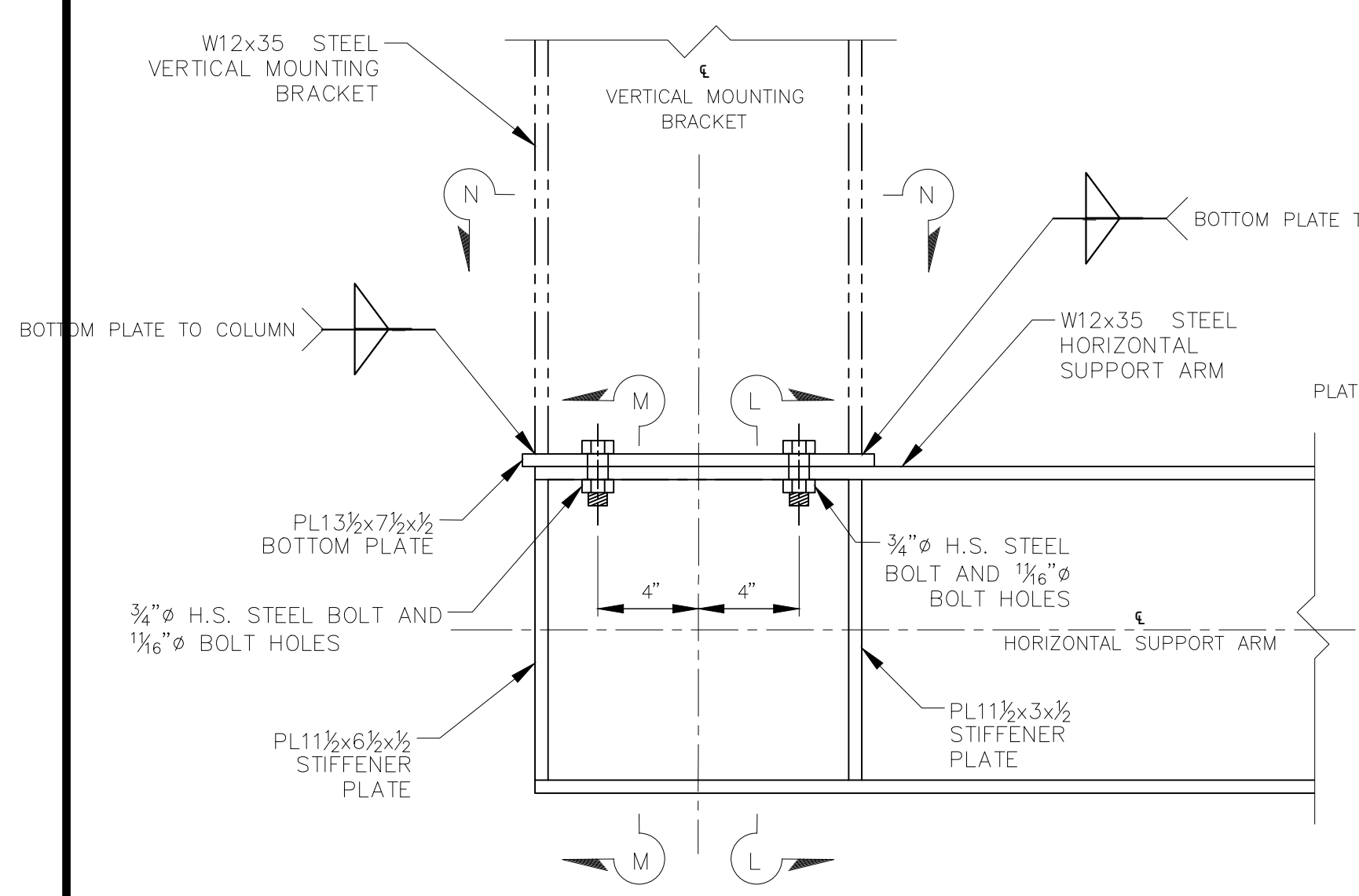


**SECTION J-J**  
SCALE: 4" = 1'-0"

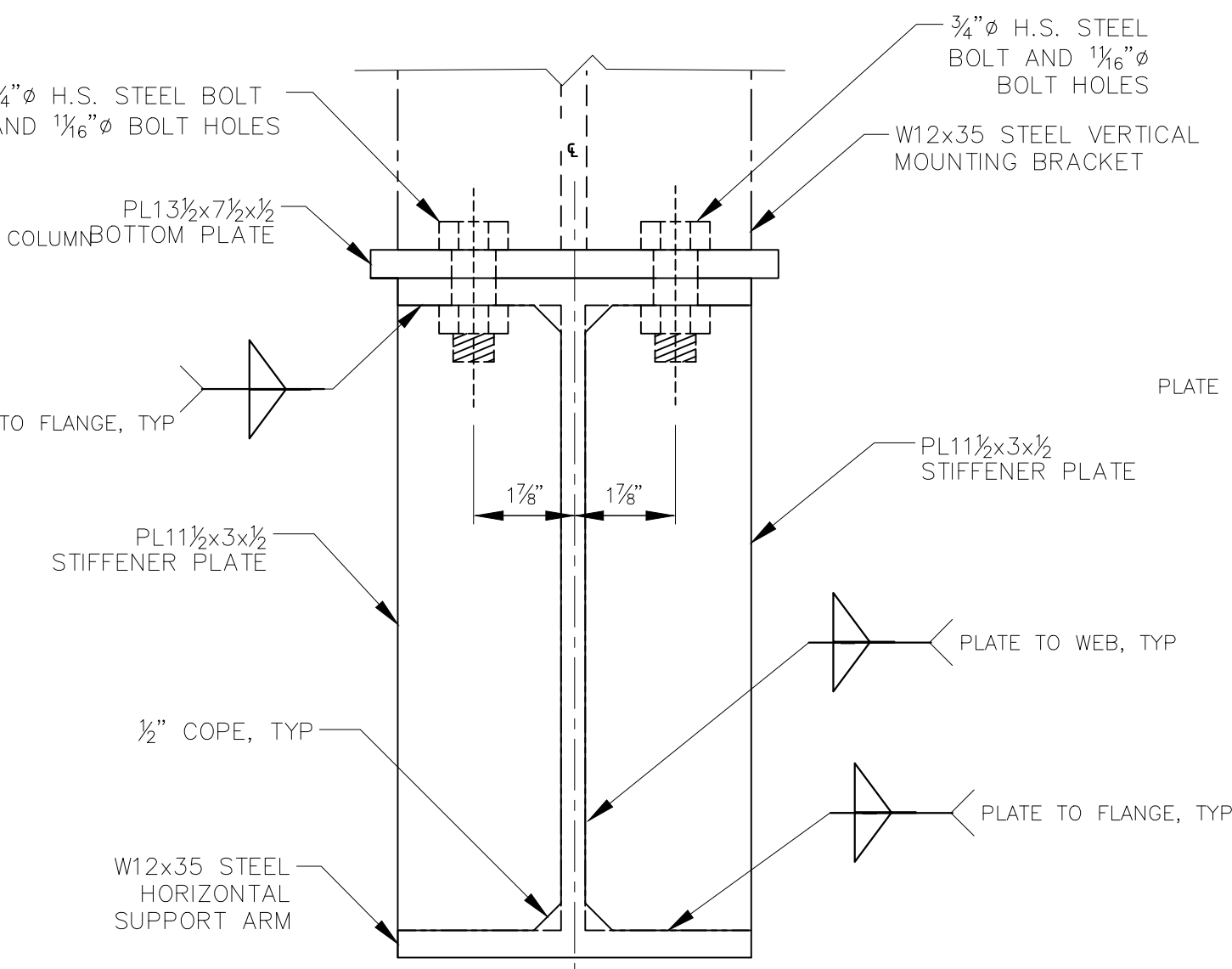


**SECTION K-K**  
SCALE: 4" = 1'-0"

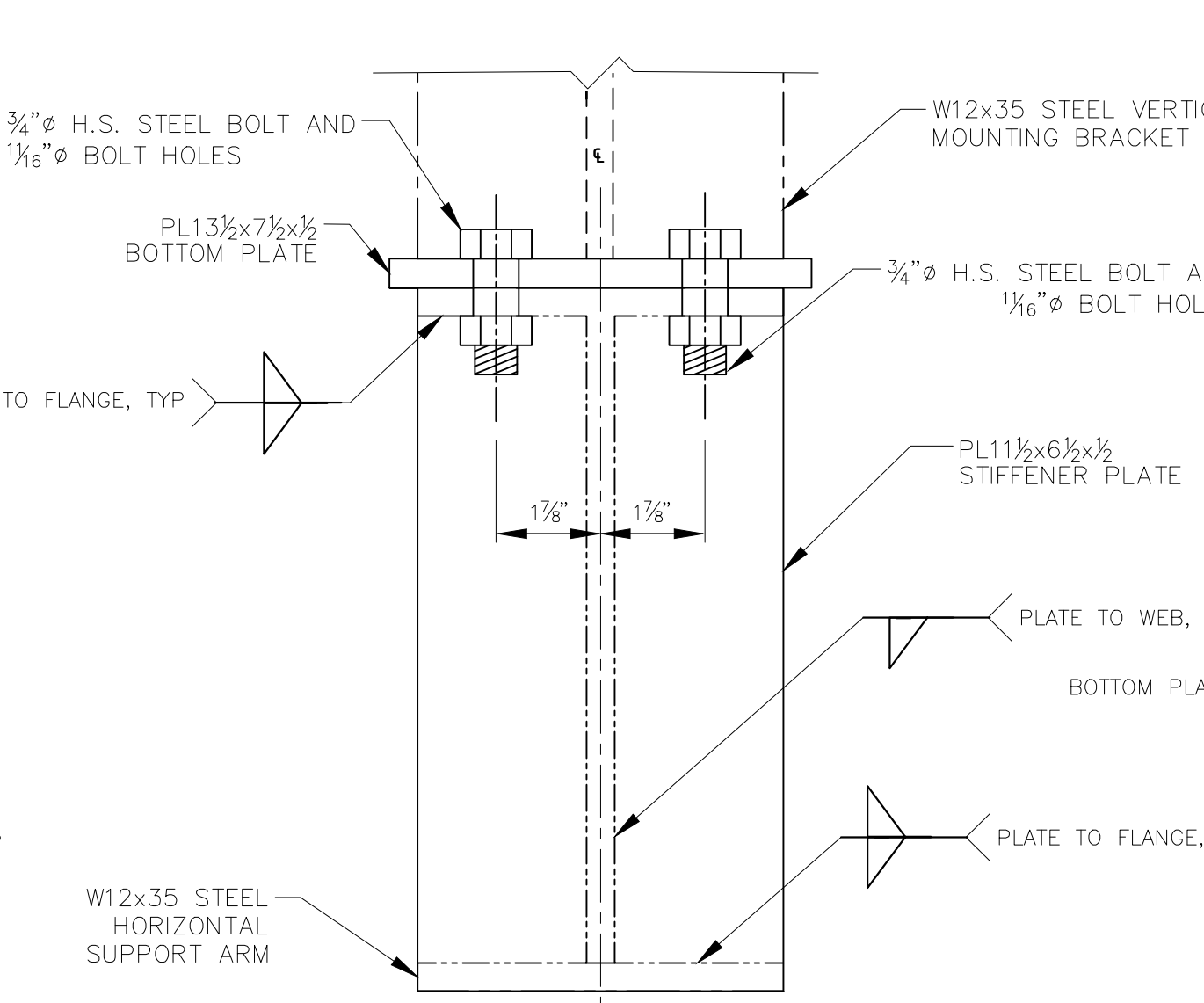
NOTE: SNUG TIGHT BOLTS AT WALKWAY SUPPORT BRACKET



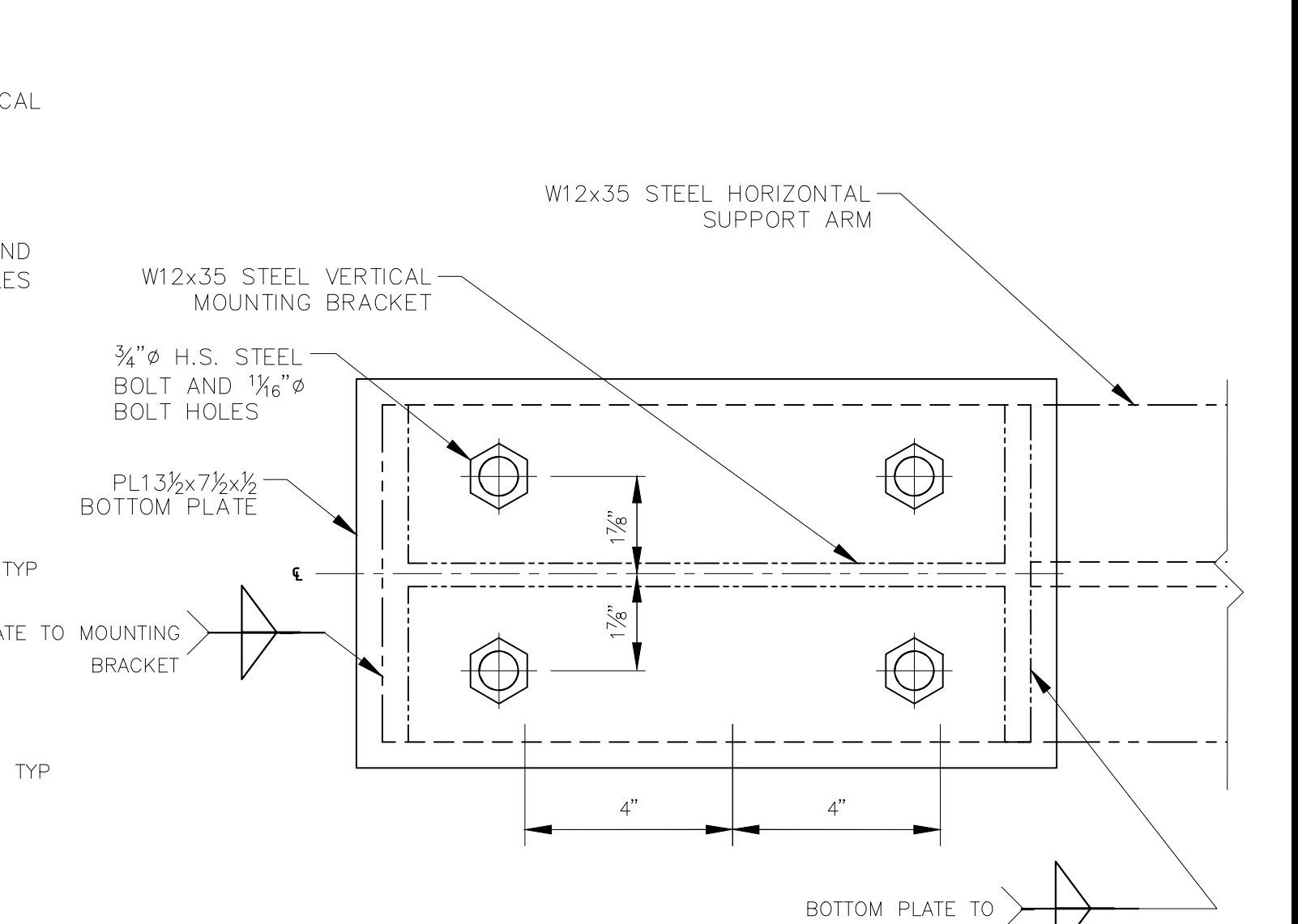
**HORIZONTAL SUPPORT ARM TO VERTICAL MOUNTING BRACKET CONNECTION DETAIL**  
SCALE: 2" = 1'-0"



**SECTION L-L**  
SCALE: 4" = 1'-0"



**SECTION M-M**  
SCALE: 4" = 1'-0"



**SECTION N-N**  
SCALE: 4" = 1'-0"

**TYPICAL COLUMN JOINT CONNECTION DETAILS**

NOTE:  
EXIST EAST COLUMN SHOWN NEW EAST COLUMN SIMILAR



**DOKKEN ENGINEERING**

1400 PRAIRIE ROAD  
SUITE 100  
SAN DIEGO, CA 92108  
(619) 514-8777

CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD

BENCH MARK  
STATION 2075 PER R08 11302 - LEAD PLUG & BRASS TAG STAMPED "RCE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.  
ELEVATION: 337.97, M.S.L. DATUM

SCALE  
HORIZ: 1" = 5'  
VERT: N/A

DESIGNED BY	DRAWN BY	CHECKED BY
CHARLES TORNACI	ERIC PONCE	CHARLES TORNACI
PLANS PREPARED UNDER THE SUPERVISION OF		
CHARLES TORNACI		
RCE NO. 66058	DATE 07/26/22	EXPIRES 6/30/2024

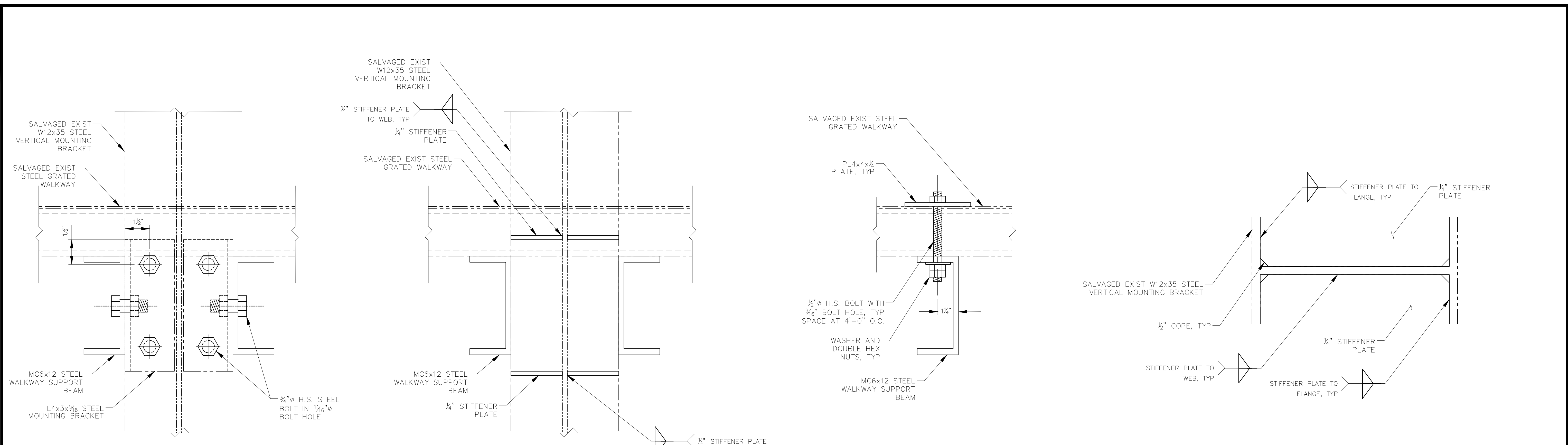
REVIEWED  
BY PROJECT ENGINEER

CITY OF SANTEE DEPARTMENT OF DEVELOPMENT SERVICES  
RELOCATION DETAILS NO. 2  
CUYAMACA STREET RIGHT TURN POCKET

CITY W.O. NO. CIP 2014-02  
DRAWING NO. 2021-119  
SHEET 20 OF 21

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 07/26/2022



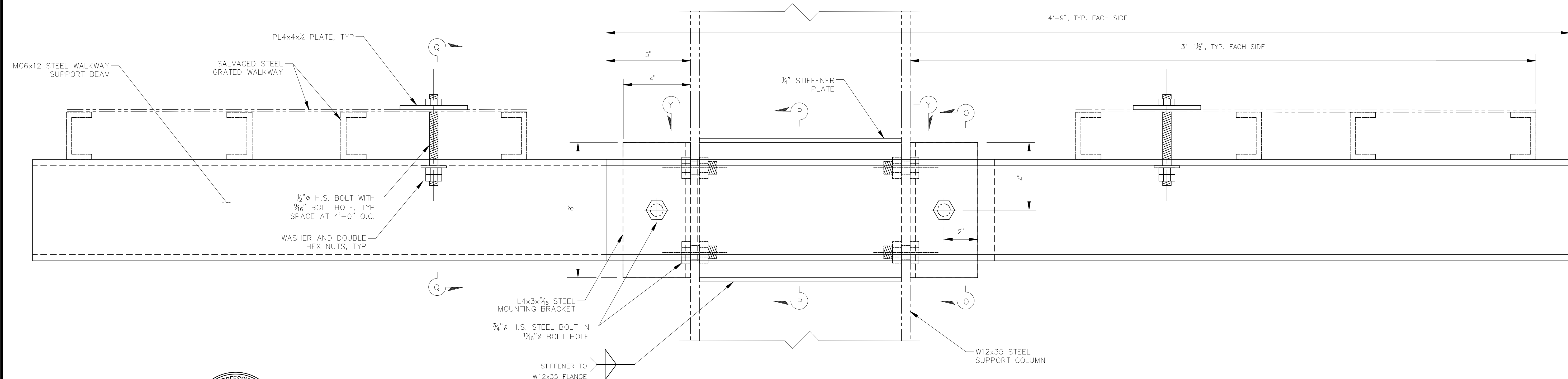


SECTION O-O  
SCALE: 4" = 1'-0"

SECTION P-P  
SCALE: 4" = 1'-0"

SECTION Q-Q  
SCALE: 4" = 1'-0"

SECTION Y-Y  
SCALE: 4" = 1'-0"



WALKWAY CONNECTION DETAIL  
SCALE: 4" = 1'-0"



CONTRACTOR  
INSPECTOR  
DATE COMPLETED

CONSTRUCTION RECORD	REFERENCES	DATE	BY	REVISIONS	ACPTD

BENCH MARK  
STATION 2075 PER RDS 11352 - LEAD PLUG & BRASS TAG STAMPED "RICE 16930" PER MAP 9372, LOCATED AT THE SOUTHEAST CORNER AT THE INTERSECTION OF MISSION GORGE ROAD AND OLIVE LANE.  
ELEVATION: 337.97, M.S.L. DATUM

SCALE  
HORIZ: 1" = 5'  
VERT: N/A

DESIGNED BY	DRAWN BY	CHECKED BY
CHARLES TORNACI	ERIC PONCE	CHARLES TORNACI
PLANS PREPARED UNDER THE SUPERVISION OF		
CHARLES TORNACI		
RCE NO. 66058	DATE 07/26/22	EXPIRES 6/30/2024

REVIEWED  
BY PROJECT ENGINEER

CITY OF SANTEE DEPARTMENT OF DEVELOPMENT SERVICES  
RELOCATION DETAILS NO. 3  
CUYAMACA STREET RIGHT TURN POCKET

CITY W.O. NO. CIP 2014-02  
DRAWING NO. 2021-120  
SHEET 21 OF 21

100% ROADWAY PS&E PLANS - CUYAMACA STREET RIGHT TURN POCKET, CIP 2014-02, 07/26/2022